



**JEFFERSON COUNTY, TEXAS
PURCHASING DEPARTMENT**

1149 Pearl Street – First Floor
Beaumont, Texas 77701
409-835-8593 phone

ADDENDUM TO RFQ

RFQ Number: RFQ 19-024/JW
RFQ Title: “On Call” Professional Airport Planning Services, Airport Master Plan Update, and Environmental Analysis for the Jack Brooks Regional Airport
RFQ Due: **11:00 am CT, Tuesday, April 30, 2019**
Addendum No.: 1
Issued (Date): April 24, 2019

TO BIDDER: This Addendum is an integral part of the RFQ package under consideration by you as a Bidder in connection with the subject matter herein identified. Jefferson County deems all sealed proposals to have been proffered in recognition and consideration of the entire RFQ package – **including all addenda**. For purposes of clarification, **receipt of this present Addendum by a Bidder should be evidenced by returning it (signed) as part of the Bidder’s sealed proposal**. If the Proposal has already been received by the Jefferson County Purchasing Department, Bidder should return this addendum in a separate sealed envelope, clearly marked with the RFQ Title, RFQ Number, and Opening Date and Time, as stated above.

Reason for Issuance of this addendum:

- **Answers to Questions Received from Potential Respondents**
- **Additional Information: DBE Goals, Master Plan Initial Needs Determination, Existing Airport Layout Drawing, Property Map, Passenger/Operations/Air Cargo Data, Wildlife Hazard Management Plan**

The information included herein is hereby incorporated into the documents of this present Bid matter and supersedes any conflicting documents or portion thereof previously issued.

Receipt of this Addendum is hereby acknowledged by the undersigned Bidder:

ATTEST:

Witness

Witness

Approved by _____ Date: _____

Authorized Signature (Bidder)

Title of Person Signing Above

Typed Name of Business or Individual

Address

(RFQ 19-024/JW) Addendum No. 1 - Questions & Answers

Question: Do the required forms in the RFQ specifications (Pages 17-21: Vendor References, Signature Page, House Bill 89 Verification, Senate Bill 252 Certification, Respondent's Certification) count towards the 30-page response limit noted in Section 5: Submittal Content?

Answer: No.

Question: Section 5: Submittal Content of the RFQ specifications states that the submittal shall include an acknowledgement of compliance with the applicable DBE and civil rights policies. Is there a DBE goal or DBE Certification Form?

Answer: Yes, there is a DBE goal. Please see Page 3 of this Addendum.

No, there is not a DBE Certification Form

SECTION 26.45: OVERALL GOAL CALCULATION

Name of Recipient: Jack Brooks Regional Airport

Goal Period: FY-2019 – 2021 (October 1, 2018 through September 30, 2021)

The Jack Brooks Regional Airport has calculated year-by-year Disadvantaged Business Enterprise (DBE) goals for 2019, 2020, and 2021. The goal calculations are attached to this overall goal summary. Each individual year's anticipated DOT-assisted contract amounts and DBE goals are represented below and are included in the calculation documentation for each individual year in the next three attachments. The overall DBE goal for the next three year period is an average of these goals and is set at 2.00% percent (median goal).

FY 2019 Anticipated DOT-assisted contract amount: \$300,000

DBE Goal: 2.00%

Design/Administration/Support = \$300,000 (100%)

Construction = \$0

Total dollar amount to be expended on DBE's: \$6,000

Design/Administration/Support = \$6,000 (2.00%)

Construction = \$0 (0%)

Describe the Number and Type of Projects for DBE goal calculations:

FY19: Design Taxiway A (RW 12 to TW B)

FY19: Design Lighting System Upgrade

FY19: Design Passenger Boarding Bridge

FY19: Design Vault Repackage

FY19: Planning Master Plan for Primary Shift

FY 2020 Anticipated DOT-assisted contract amount: \$1,000,000

DBE Goal: 4.81%

Design/Administration/Support = \$0 (0%)

Construction = \$1,000,000 (100%)

Total dollar amount to be expended on DBE's: \$48,100

Design/Administration/Support = \$0 (0%)

Construction = \$48,100 (100%)

Describe the Number and Type of Projects for DBE goal calculations:

FY20: Construction Taxiway A (RW 12 to TW B)

FY20: Construction Lighting System



Jack Brooks Regional Airport (BPT) Master Plan Update Initial Needs Determination January 2019

Executive Summary

Jack Brooks Regional Airport (BPT) is part of the Federal Aviation Administration (FAA) National Plan of Integrated Airport Systems (NPIAS). BPT is categorized as a Non-hub commercial service airport with annual enplanements that range between 23,000 and 26,000 passengers per year.

BPT is an essential mode of transportation in Jefferson County, Texas, the surrounding counties, and the Southeastern portions of the Texas Gulf Coast. Jefferson County is part of the Beaumont – Port Arthur, Texas metropolitan area. According to the 2015 US Census the estimated population of Jefferson County is 254,308.

BPT serves an important role in the NPIAS since it is the only commercial service airport in Southeast Texas. BPT is owned and operated by Jefferson County, Texas. BPT is located southwest of the city of Nederland, Texas in an unincorporated area of Jefferson County, Texas. BPT is currently served by Envoy Air Inc. (formerly American Eagle) on behalf of American Airlines.

Jefferson County will need to conduct a comprehensive study of the airport to evaluate and prepare for the short, medium, and long-term developmental needs of the airfield as well as future aviation demand. The current FAA approved Airport Master Plan was completed in 2007. A Master Plan Update with all applicable sections of FAA AC 150/5070-6B Airport Master Plans is needed. A Master Plan Update must also place emphasis on ensuring the airport is enhanced in terms of all applicable sections of FAA AC 150/5300-13A Airport Design.

This initial needs determination document lists the various factors that justify the need for an Airport Master Plan Update. This document will also assist Jefferson County with developing the scope for the Airport Master Plan Update as well as assist the County with the preparation of the Request for Qualifications for a potential consulting and planning firm to undertake the study.

Needs Determination and reasons for an Airport Master Plan Update

1.0 Potential changes in the air carrier aircraft fleet within the next 5 – 7 years

On November 13, 2015, Envoy began accepting deliveries on a new Embraer E175 (76 passenger) aircraft with 90 configuration options. Envoy took delivery of an additional two Embraer E175 in October 2016. In May 2018 they announced they would take



delivery of 15 additional E175s and in November 2018 they announced they would take and additional 15 E175, giving them a total of 84 E175s by mid-2020.

Jefferson County would benefit from these larger aircraft and total enplanements could increase over the next 5 – 7 years. Therefore, an Airport Master Plan Update should research and evaluate airfield changes that would accommodate a potential change of aircraft equipment.

Some of the local aviation tenants may also purchase new jet aircraft (an example aircraft type is the Cessna Citation XL) and potentially increase their operations within the next 5 – 7 years.

2.0 Jefferson County Diverse Economy and Aviation Opportunities

BPT strategic goal is to continue to be an essential part of the National transportation system because of the direct and indirect connectivity to:

1. Interstate Highway 10
2. US Highways 90 and 69-96-287
3. State Highways 73, 87, and 105 and 3 farm-to-market roads
4. Rail and motor freight carriers
5. Deep-water ports located in:
 - a. Beaumont, Texas
 - b. Port Arthur, Texas
 - c. Orange, Texas
 - d. Sabine Pass, Texas

Note: The Sabine Neches Waterway is the 3rd largest port in the United States by tonnage and offers deep-water access to ocean-going vessels.

BPT is uniquely located to efficiently serve the residents and visitors of Jefferson County, the surrounding counties, and the vibrant industrial and petrochemical companies in the area. Corporations in the county are involved in the refining of petroleum, the production of bio-fuels and other chemicals, steel fabrication, agricultural products, and health care services. Some refineries have recently announced major plant expansions within the next 1 - 5 years. Notable corporations in the Jefferson County area include:

- Air Products
- Exxon Mobil
- Golden Pass LNG Terminal
- Lucite International, LLC
- OCI Beaumont, LLC
- Phillips 66 Beaumont Terminal
- The Vitol Group
- Sempra Energy



3.0 Airport Strategic Vision

Jefferson County's strategic vision for the airport is to increase the number of annual enplaned passengers and increase aircraft operations while continuing to improve the safety and efficiency of the airfield. The vision also includes continuing the self-sufficiency of the airport by creating more opportunities to increase both airside and landside revenue streams.

Jefferson County can achieve this vision by changing the airfield geometry in a manner that enhances airport safety and efficiently in an environmentally responsible manner. Jefferson County also needs to ensure the airfield meets all applicable FAA standards to the extent practical. An Airport Master Plan Update is necessary to help BPT do the necessary strategic planning to achieve the following objectives (as a minimum):

3.1 Standardizing the airfield geometry will ensure BPT is prepared for FAA NextGEN capabilities and technologies where applicable. This includes improving the airport surface by eliminating the intersection of the runways at BPT.

3.2 BPT can potentially increase safety both on and off the airfield by eliminating the overlapping runways and by shifting the RPZs onto airport property.

3.3 BPT can potentially improve the airport geometry so that runway and taxiway components fully match current FAA standards and FAA advisory circular AC 150/5300-13A – Airport Design. This includes addressing the long standing problem of intersecting runways and fixing the taxiway Foxtrot connection to runway 34.

3.4 BPT can potentially create a configuration that would be less confusing to itinerant pilots and reduce the possibility of wrong runway departure by shifting the thresholds of runways 16/34 and 12/30. BPT currently has sufficient land within the airport boundary to shift the crossing runways beyond the current intersection point without overlapping newly created runway safety areas (RSA).

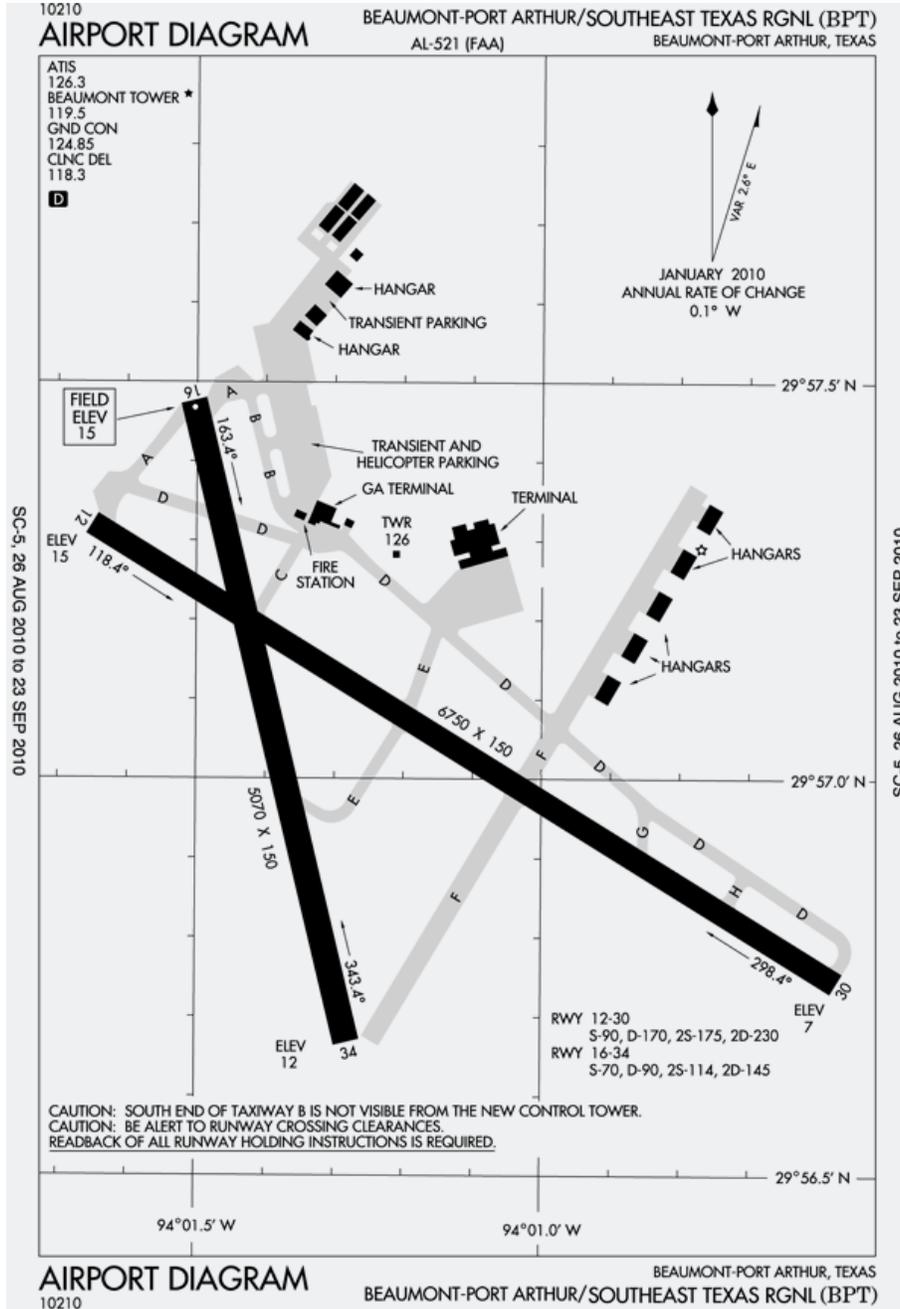
3.5 BPT can potentially reduce the possibility of runway incursions by shifting the thresholds of runways 16/34 and 12/30. This shift would eliminate an ATCT "blind spot" on a portion of taxiway Bravo adjacent to the GA ramp. This "blind spot" was identified in a recent BPT Runway Safety Action Team (RSAT) meeting. This shift will also give BPT an opportunity to create new and understandable pavement signs and markings.

3.6 BPT can protect future approaches as well as prevent incompatible land uses if a new Airport Layout Drawing (ALD) is developed as part of a Master Plan Update.

3.7 BPT can potentially develop a plan that would address the current age and condition of the existing airfield pavements.



3.8 BPT could potentially shift the noise contours away from residential areas west and northwest of the airfield.



4.0 Airfield development opportunities due to existing compatible land uses



BPT is located on approximately 1180 acres, and is currently surrounded by compatible land uses including residential, light commercial, and light industrial real estate and properties. BPT periodically receives requests for routing of utilities to service residential areas as well as requests for new pipelines that serve the petrochemical industries near the airport.

BPT is responsible for protecting the airfield imaginary surfaces described in 14 CFR Part 77 — Safe, Efficient Use, and Preservation of Navigable Airspace. However, protecting the airspace any future airfield developmental improvements is not possible without a new Airport Layout Plan (ALP) and a new Airport Layout Drawing (ALD) depicting the needed improvements.

Updating the Airport Master Plan at this time will help Jefferson County make the appropriate decisions and determinations when various entities seek to use land adjacent to the airport. A BPT Airport Master Plan Update will also need to include the development of a new ALP.

5.0 Emergency planning, response, and partnerships

BPT is a participating member of the Southeast Airport Disaster Operations Group (SEADOG). BPT has partnered with other SEADOG airports along the Gulf Coast and the Atlantic Ocean to receive assistance as well as offer support to airports that are impacted by natural disasters such as hurricanes or floods. During a crisis, BPT can offer specific expertise and well as temporary storage or staging for relief operations. A comprehensive airport planning effort can potentially identify effective staging areas for emergency operations.

6.0 Summary and Needs Determination

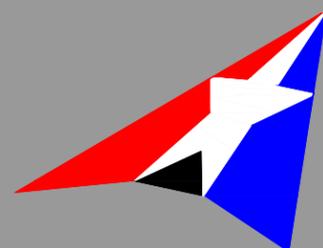
The existing Airport Master Plan was completed in 2007. For this reason and for reasons stated in this document, it has been determined that an Airport Master Plan Update is needed at Jack Brooks Regional Airport.

Jefferson County, Texas should seek a qualified consulting firm or team to provide professional on-call airport planning services to develop a comprehensive **Airport Master Plan Update and related Environmental Analysis** for Jack Brooks Regional Airport (BPT). See Table 1 for an approximate RFQ solicitation award schedule and timeline.

Table 1
Proposed schedule for the RFQ solicitation and potential award of an
“On-Call” Airport Planning Consultant or Team

BPT Proposed RFQ Solicitation and Award Timeline		
Task	Date*	Remarks
1. RFQ Solicitation release date	03/25/19	
2. Non-mandatory pre-submittal meeting	04/16/19	4+ weeks at 1:30 p.m.
3. Last Day for Questions	04/26/19	
4. Submittal submission deadline	04/30/19	By 4:00 p.m.
5. Technical review of submittals	05/10/19	Round 1
6. Technical review of submittals	05/17/19***	Round 2 (if necessary)
7. All interviews scheduled	05/31/19**	
8. Award Notification (approximate)	06/28/19	
9. Execution of Agreement (approximate)	08/12/19	45 days (estimated)

- (*) = actual dates may vary per County approval process
(**) = interview scheduling may dictate actual date(s)
(***) = if necessary



Airport Layout Plan

Jack Brooks Regional Airport
Beaumont / Port Arthur, Texas

January, 2015



Sheet List Table		
Sheet Number	DRAWING NUMBER	Sheet Title
COVER		
1	COV	Cover Sheet
AIRPORT LAYOUT		
2	ALD-1	Airport Layout Drawing
3	ALD-2	Airport Layout Drawing Tables
AIRSPACE		
4	ASD	Airspace Drawing
INNER PORTION OF THE APPROACH SURFACE		
5	IPASD-12	Inner Portion of the Approach Surface - Runway 12
6	IPASD-30	Inner Portion of the Approach Surface - Runway 30
7	IPASD-16	Inner Portion of the Approach Surface - Runway 16
8	IPASD-34	Inner Portion of the Approach Surface - Runway 34
9	IAOT	Inner Approach Obstruction Tables
TERMINAL AREA		
10	TAKM	Terminal Area Key Map
11	TAD-1	Terminal Area Drawing I
12	TAD-2	Terminal Area Drawing II
13	TAD-3	Terminal Area Drawing III
LAND USE		
14	LUD	Land Use Drawing
AIRPORT PROPERTY		
15	APKM	Airport Property Key Map
16	APM-1	Airport Property Map I
17	APM-2	Airport Property Map II
18	APM-3	Airport Property Map III
19	APM-4	Airport Property Map IV
20	APM-5	Airport Property Map V
21	APM-6	Airport Property Map VI
22	APM-7	Airport Property Map VII
23	APT	Airport Property Tables



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(972) 377-7480

REGISTRATION NO.
F-5713

REV.	DATE	DESCRIPTION

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORTR ARTHUR, TEXAS

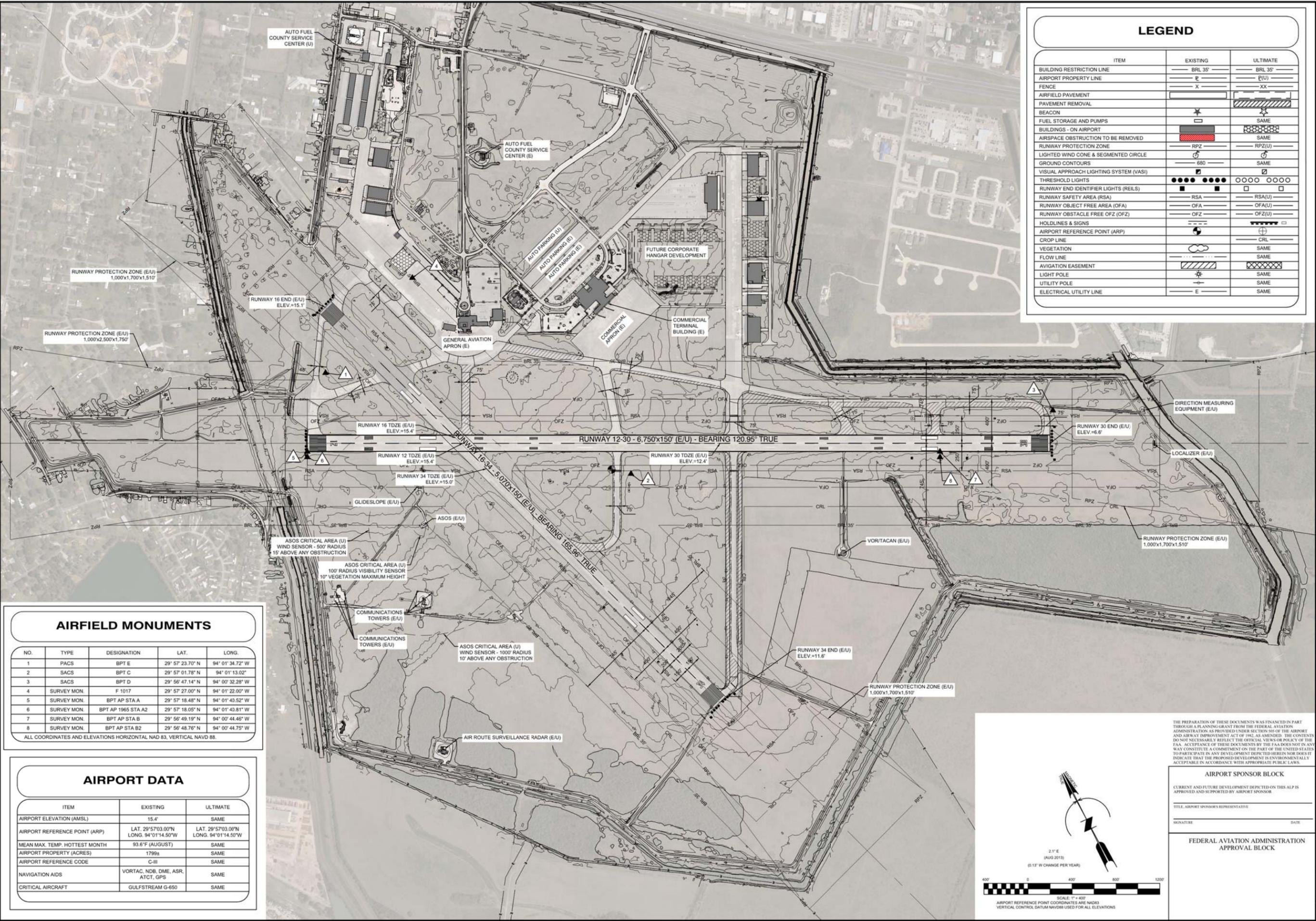
AIRPORT
LAYOUT
DRAWING

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

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IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER
ALD-1
SHEET NUMBER
2

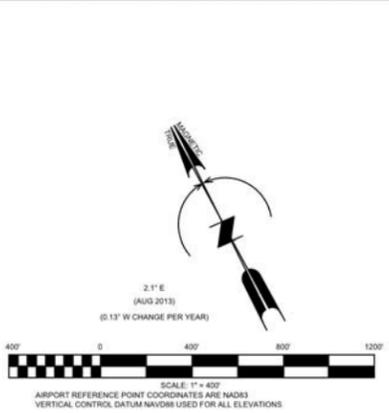
LEGEND		
ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 35'	BRL 35'
AIRPORT PROPERTY LINE	E	E(U)
FENCE	X	XX
AIRFIELD PAVEMENT	[Symbol]	[Symbol]
PAVEMENT REMOVAL	[Symbol]	[Symbol]
BEACON	[Symbol]	[Symbol]
FUEL STORAGE AND PUMPS	[Symbol]	SAME
BUILDINGS - ON AIRPORT	[Symbol]	SAME
AIRSPACE OBSTRUCTION TO BE REMOVED	[Symbol]	SAME
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	[Symbol]	[Symbol]
GROUND CONTOURS	680	SAME
VISUAL APPROACH LIGHTING SYSTEM (VASI)	[Symbol]	[Symbol]
THRESHOLD LIGHTS	[Symbol]	[Symbol]
RUNWAY END IDENTIFIER LIGHTS (REILS)	[Symbol]	[Symbol]
RUNWAY SAFETY AREA (RSA)	RSA	RSA(U)
RUNWAY OBJECT FREE AREA (OFA)	OFA	OFA(U)
RUNWAY OBSTACLE FREE OFZ (OFZ)	OFZ	OFZ(U)
HOLDLINES & SIGNS	[Symbol]	[Symbol]
AIRPORT REFERENCE POINT (ARP)	[Symbol]	[Symbol]
CROP LINE	CRL	CRL
VEGETATION	[Symbol]	SAME
FLOW LINE	[Symbol]	SAME
AVIGATION EASEMENT	[Symbol]	[Symbol]
LIGHT POLE	[Symbol]	SAME
UTILITY POLE	[Symbol]	SAME
ELECTRICAL UTILITY LINE	E	SAME



AIRFIELD MONUMENTS				
NO.	TYPE	DESIGNATION	LAT.	LONG.
1	PACS	BPT E	29° 57' 23.70" N	94° 01' 34.72" W
2	SACS	BPT C	29° 57' 01.78" N	94° 01' 13.02" W
3	SACS	BPT D	29° 56' 47.14" N	94° 00' 32.28" W
4	SURVEY MON.	F 1017	29° 57' 27.00" N	94° 01' 22.00" W
5	SURVEY MON.	BPT AP STA A	29° 57' 18.48" N	94° 01' 40.52" W
6	SURVEY MON.	BPT AP 1965 STA A2	29° 57' 18.05" N	94° 01' 40.81" W
7	SURVEY MON.	BPT AP STA B	29° 56' 49.19" N	94° 00' 44.46" W
8	SURVEY MON.	BPT AP STA B2	29° 56' 48.76" N	94° 00' 44.75" W

ALL COORDINATES AND ELEVATIONS HORIZONTAL NAD 83, VERTICAL NAVD 88.

AIRPORT DATA		
ITEM	EXISTING	ULTIMATE
AIRPORT ELEVATION (AMSL)	15.4'	SAME
AIRPORT REFERENCE POINT (ARP)	LAT. 29°57'03.00"N LONG. 94°01'14.50"W	LAT. 29°57'03.00"N LONG. 94°01'14.50"W
MEAN MAX. TEMP. HOTTEST MONTH	93.6°F (AUGUST)	SAME
AIRPORT PROPERTY (ACRES)	1799±	SAME
AIRPORT REFERENCE CODE	C-III	SAME
NAVIGATION AIDS	VORTAC, NDB, DME, ASR, ATCT, GPS	SAME
CRITICAL AIRCRAFT	GULFSTREAM G-650	SAME



THE PREPARATION OF THESE DOCUMENTS WAS FINANCED IN PART THROUGH A PLANNING GRANT FROM THE FEDERAL AVIATION ADMINISTRATION AS PROVIDED UNDER SECTION 505 OF THE AIRPORT AND AIRWAY IMPROVEMENT ACT OF 1982, AS AMENDED. THE CONTENTS DO NOT NECESSARILY REFLECT THE OFFICIAL VIEWS OR POLICY OF THE FAA. ACCEPTANCE OF THESE DOCUMENTS BY THE FAA DOES NOT IN ANY WAY CONSTITUTE A COMMITMENT ON THE PART OF THE UNITED STATES TO PARTICIPATE IN ANY DEVELOPMENT DEPICTED HEREIN NOR DOES IT INDICATE THAT THE PROPOSED DEVELOPMENT IS ENVIRONMENTALLY ACCEPTABLE IN ACCORDANCE WITH APPROPRIATE PUBLIC LAWS.

AIRPORT SPONSOR BLOCK
CURRENT AND FUTURE DEVELOPMENT DEPICTED ON THIS ALP IS APPROVED AND SUPPORTED BY AIRPORT SPONSOR.
TITLE AIRPORT SPONSOR'S REPRESENTATIVE
SIGNATURE: _____ DATE: _____

FEDERAL AVIATION ADMINISTRATION
APPROVAL BLOCK

File: L:\2015\12121740 - BPT ALP Update & Obstruction Survey\Drawings\BPT-ALD-002.dwg, Last Save: 1/5/2015 2:18 PM Last saved by: Jahenderson
Last plotted by: Henderson, James, A Plot Style: AECmonochrome.ctb Plot Scale: 1:1 Plot Date: 1/5/2015 2:23 PM Plotter used: DWG To PDF.pc3



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REGISTRATION NO.
F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORARTHUR, TEXAS

AIRPORT LAYOUT PLAN

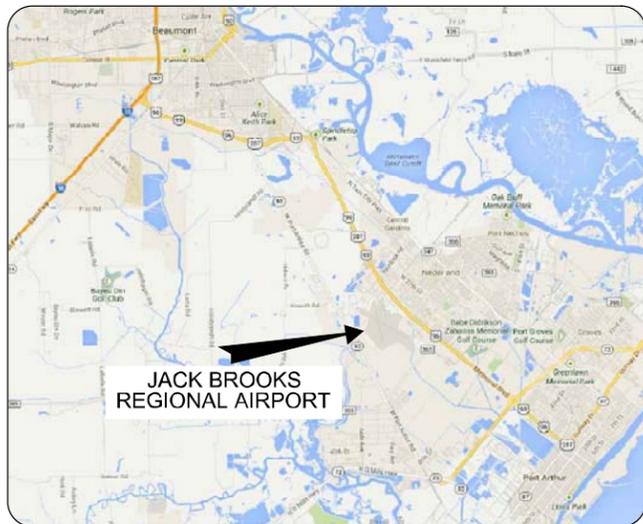
AIRPORT LAYOUT DRAWING TABLES

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

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IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER
ALD-2
SHEET NUMBER
3

VICINITY MAP



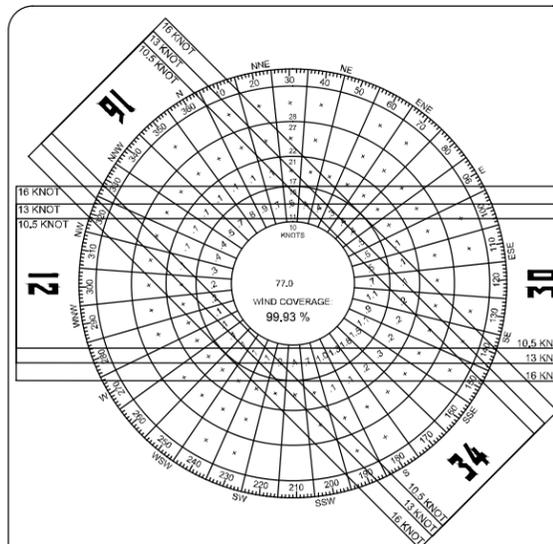
APPROACH AND THRESHOLD SITING SURFACE PENETRATIONS

SEE SHEET 4 AIRSPACE DRAWING AND SHEETS 5-8 INNER PORTION OF THE APPROACH SURFACE DRAWINGS FOR APPROACH AND THRESHOLD SITING SURFACE PENETRATIONS.

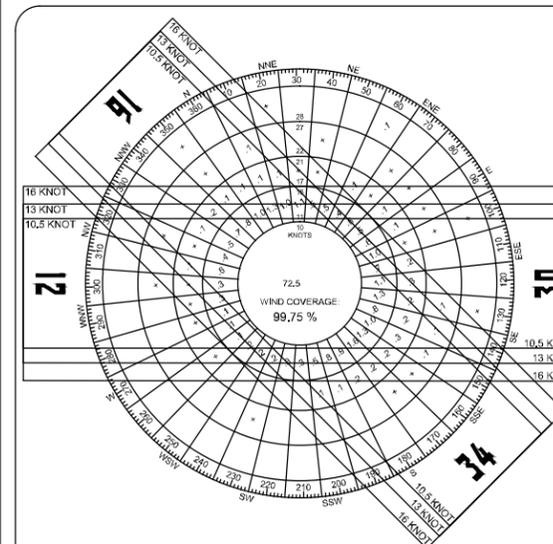
OBSTACLE FREE ZONE

NO EXISTING OBSTACLE FREE ZONE PENETRATIONS

ALL WEATHER WIND ROSE



IFR WIND ROSE



RUNWAY DATA

ITEM	RUNWAY 12/30		RUNWAY 16/34	
	EXISTING	ULTIMATE	D / D	SAME
APPROACH VISIBILITY MINIMUMS	PIR / D	SAME	D / D	SAME
PART 77 APPROACH SURFACES	50:1 / 34:1	SAME	34:1 / 34:1	SAME
FAR PART 77 CATEGORY	PIR / D	SAME	D / D	SAME
RUNWAY WIDTH AND LENGTH	6,750' X 150'	SAME	5,070' X 150'	SAME
PAVEMENT TYPE	CONCRETE - GRVD	SAME	CONCRETE - GRVD / ASPH	CONCRETE - GRVD
PAVEMENT STRENGTH	90,000 LBS SWG	SAME	70,000 LBS SWG	SAME
RUNWAY LIGHTING	HIRL	SAME	HIRL	SAME
RUNWAY MARKING	PIR	SAME	NPI	SAME
EFFECTIVE RUNWAY GRADIENT %	0.1%	SAME	0.1%	SAME
MAXIMUM GRADE WITHIN RUNWAY LENGTH	0.1%	SAME	0.1%	SAME
RUNWAY LINE-OF-SIGHT	CRITERIA MET	SAME	CRITERIA MET	SAME
VISUAL APPROACH AIDS	MALS/R / VASI-4L	SAME	VASI-4L	SAME
INSTRUMENT APPROACH AIDS	I.L. RNAV / RNAV	SAME	RNAV	SAME
AIRPORT REFERENCE CODE (ARC)	C-II	SAME	B-II	SAME
RUNWAY SAFETY AREA	500' X 8,750'	SAME	300' X 6,270'	SAME
RUNWAY OBJECT FREE AREA	800' X 8,750'	SAME	800' X 6,270'	SAME
OBSTACLE FREE ZONE	800' X 7,150'	SAME	400' X 5,470'	SAME
TAXIWAY MARKINGS	STANDARD/CENTERLINE	SAME	STANDARD/CENTERLINE	SAME
TAXIWAY LIGHTING	MITL	SAME	MITL	SAME
TAXIWAY WIDTH	75'	SAME	75'	SAME
% WIND COVERAGE	99.93%	SAME	96.93%	SAME
AERONAUTICAL SURVEY TYPE	18B	-	18B	-

RUNWAY DATA TOUCHDOWN ZONE ELEVATION (TDZE)

RUNWAY	EXISTING	ULTIMATE
R/W 12	15.4'	SAME
R/W 30	12.4'	SAME
R/W 16	15.4'	SAME
R/W 34	15.0'	SAME

NOTE: TDZE REPRESENTS THE HIGHEST ELEVATION WITHIN THE FIRST 3000' FROM A RUNWAY END.

RUNWAY END COORDINATES

RUNWAY END	EXISTING COORDINATES	EXISTING ELEVATION	ULTIMATE COORDINATES	ULTIMATE ELEVATION
R/W 12	LAT. 29° 57' 18.11" N LONG. 94° 01' 40.07" W	15.2'	LAT. 29° 57' 18.11" N LONG. 94° 01' 40.07" W	SAME
R/W 30	LAT. 29° 58' 44.74" N LONG. 94° 00' 34.27" W	6.6'	LAT. 29° 58' 44.74" N LONG. 94° 00' 34.27" W	SAME
R/W 16	LAT. 29° 57' 28.76" N LONG. 94° 01' 31.29" W	15.1'	LAT. 29° 57' 28.76" N LONG. 94° 01' 31.29" W	SAME
R/W 34	LAT. 29° 58' 40.07" N LONG. 94° 01' 17.31" W	11.6'	LAT. 29° 58' 40.07" N LONG. 94° 01' 17.31" W	SAME

RUNWAY DATA DECLARED DISTANCES

	RUNWAY 12/30		RUNWAY 16/34	
	EXISTING	ULTIMATE	EXISTING	ULTIMATE
TAKEOFF RUN AVAILABLE (TORA)	6,750'/6,750'	SAME	5,070'/5,070'	SAME
TAKEOFF DISTANCE AVAILABLE (TODA)	6,750'/6,750'	SAME	5,070'/5,070'	SAME
ACCELERATE-STOP DISTANCE AVAILABLE (ASDA)	6,750'/6,750'	SAME	5,070'/5,070'	SAME
LANDING DISTANCE AVAILABLE (LDA)	6,750'/6,750'	SAME	5,070'/5,070'	SAME

NON-STANDARD CONDITIONS

ITEM	AIRPORT REFERENCE CODE		STANDARD		NON-STANDARD CONDITION		REMARKS
	EXISTING	ULTIMATE	EXISTING	ULTIMATE	EXISTING	ULTIMATE	
NO NON-STANDARD CONDITIONS EXIST							

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AIRPORT SPONSOR BLOCK
CURRENT AND FUTURE DEVELOPMENT PROPOSED ON THIS AIRPORT APPROVED AND SUPPORTED BY AIRPORT SPONSOR:
TITLE: AIRPORT SPONSOR'S SIGNATURE
SIGNATURE DATE

FEDERAL AVIATION ADMINISTRATION APPROVAL BLOCK

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REGISTRATION NO.
F-5713

REV.	DATE	DESCRIPTION

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS
AIRPORT LAYOUT PLAN

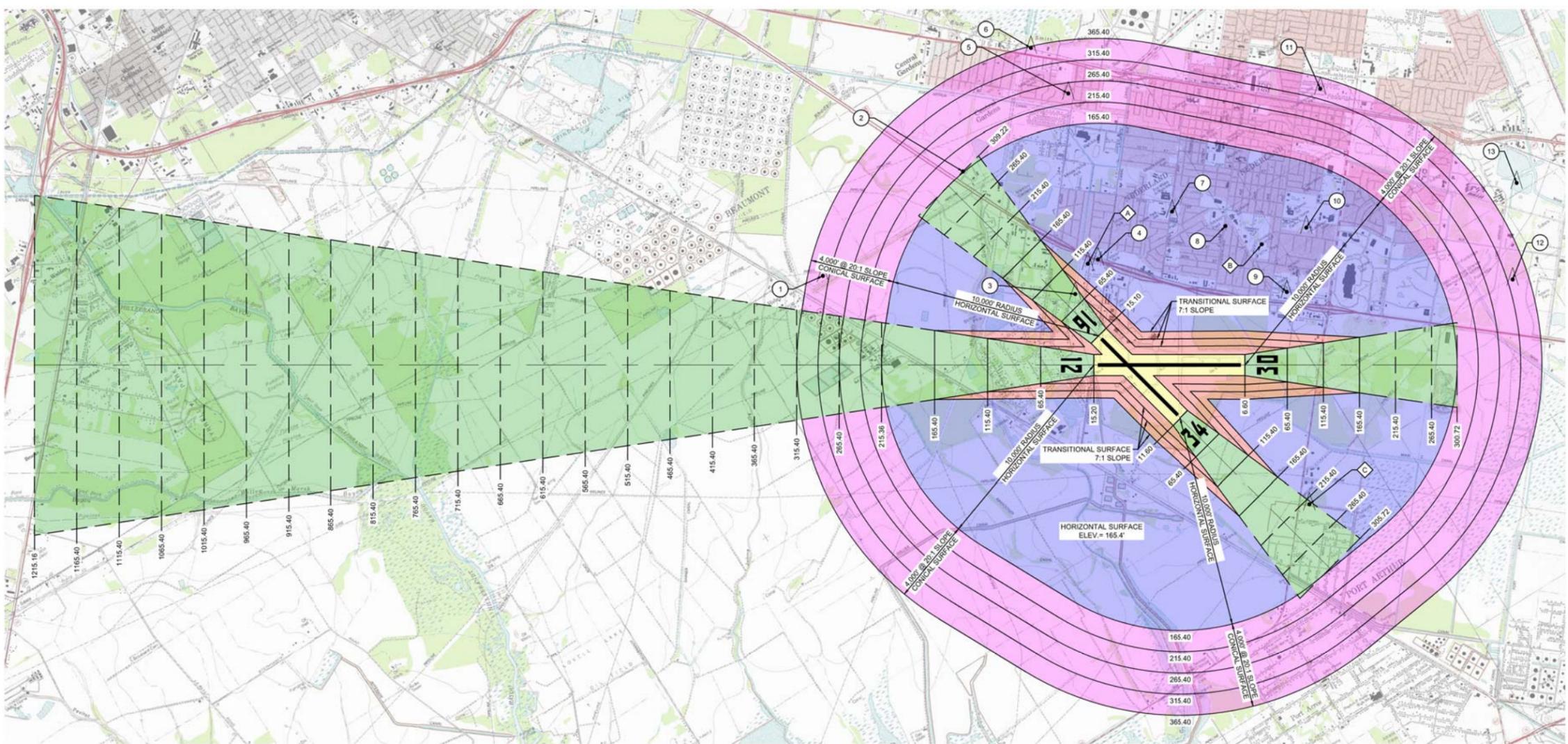
AIRSPACE
DRAWING

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

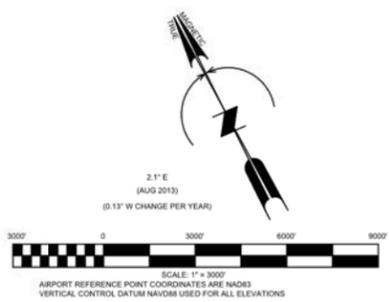
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DRAWING NUMBER
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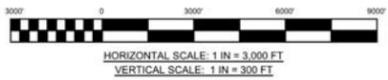
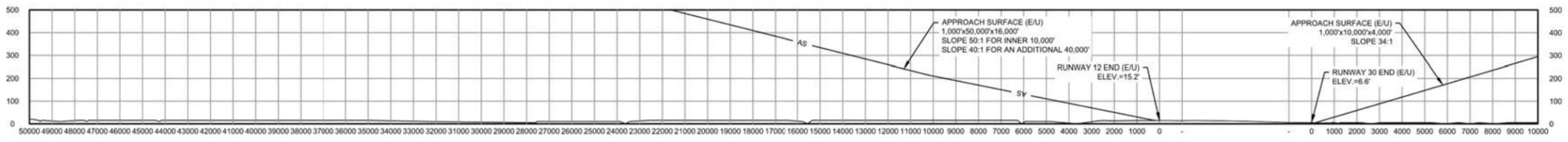
SHEET NUMBER
4



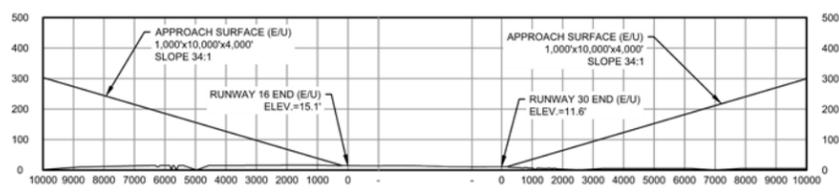
- LEGEND**
- ZONE A: PRIMARY SURFACE
 - ZONE B: APPROACH SURFACE
 - ZONE C: TRANSITIONAL SURFACES
 - ZONE D: HORIZONTAL SURFACE
 - ZONE E: CONICAL SURFACES



RUNWAY 12-30 PROFILE



RUNWAY 16-34 PROFILE



AIRSPACE OBSTRUCTION/DISPOSITION TABLE

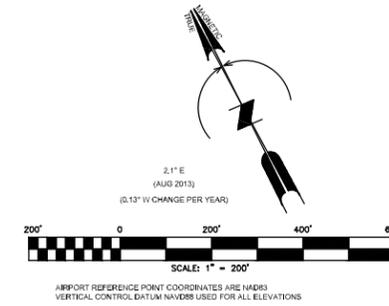
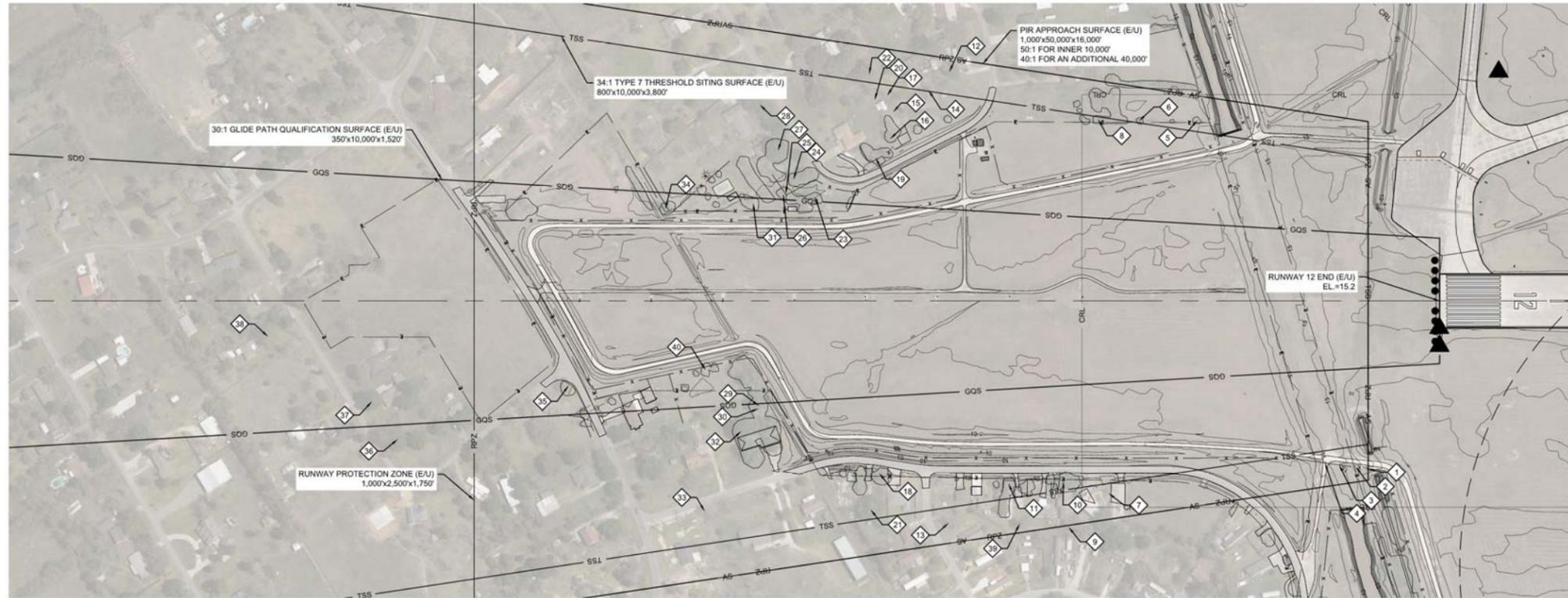
DESCRIPTION	TOP ELEVATION	SURFACE PENETRATED	PENETRATION IN FEET	LOCATION	FAA STUDY NUMBER	DISPOSITION
⬠ LIGHTED TOWER	170' (152' AGL)	HORIZONTAL	4.6'	29° 58' 01.40"N, 94° 01' 16.90"W	2009ASW07100E	NONE
⬠ LIGHTED TANK	170' (160' AGL)	HORIZONTAL	4.6'	29° 57' 02.88"N, 93° 59' 26.21"W	2010ASW051760E	NONE
⬠ TOWER	188' (185' AGL)	HORIZONTAL	22.6'	29° 54' 13.00"N, 94° 00' 41.00"W	2003ASW057370E	NONE

AIRSPACE CLEARANCE TABLE

DESCRIPTION	TOP ELEVATION	SURFACE	CLEARANCE IN FEET	LOCATION	FAA STUDY NUMBER
① TOWER	158' (141' AGL)	CONICAL	172.3'	29° 58' 16.00"N, 94° 03' 22.00"W	0069_HU005220E
② POLE	141' (126' AGL)	HORIZONTAL	24.4'	29° 59' 00.85"N, 94° 01' 48.80"W	2007ASW008610E
③ TOWER	75' (60' AGL)	APPROACH	2.9'	29° 57' 44.00"N, 94° 01' 31.00"W	0066_HU007080E
④ TOWER	129' (114' AGL)	HORIZONTAL	36.4'	29° 57' 53.00"N, 94° 01' 11.00"W	1973ASW10930E
⑤ TOWER	120' (100' AGL)	CONICAL	120.1'	29° 59' 09.00"N, 94° 00' 39.00"W	-
⑥ TOWER	208' (189' AGL)	CONICAL	153.7'	29° 59' 14.76"N, 94° 00' 43.63"W	2009ASW033000E
⑦ TOWER	161' (145' AGL)	HORIZONTAL	4.4'	29° 58' 02.10"N, 94° 00' 23.80"W	2007ASW097560E
⑧ RIG	161' (151' AGL)	HORIZONTAL	4.4'	29° 57' 28.00"N, 94° 00' 03.00"W	-
⑨ TOWER	131' (125' AGL)	HORIZONTAL	34.4'	29° 56' 51.31"N, 93° 59' 52.92"W	2011ASW034530E
⑩ TOWER	165' (155' AGL)	HORIZONTAL	0.4'	29° 57' 02.88"N, 93° 59' 26.21"W	2010ASW051760E
⑪ TOWER	172' (159' AGL)	CONICAL	167.4'	29° 58' 02.76"N, 93° 58' 41.62"W	2011ASW014680E
⑫ T-L TOWER	107' (105' AGL)	CONICAL	220.8'	29° 55' 43.16"N, 93° 58' 05.74"W	2009ASW012220E
⑬ T-L TOWER	110' (105' AGL)	CLEAR OF ALL	-	29° 56' 13.65"N, 93° 57' 37.27"W	2009ASW012130E

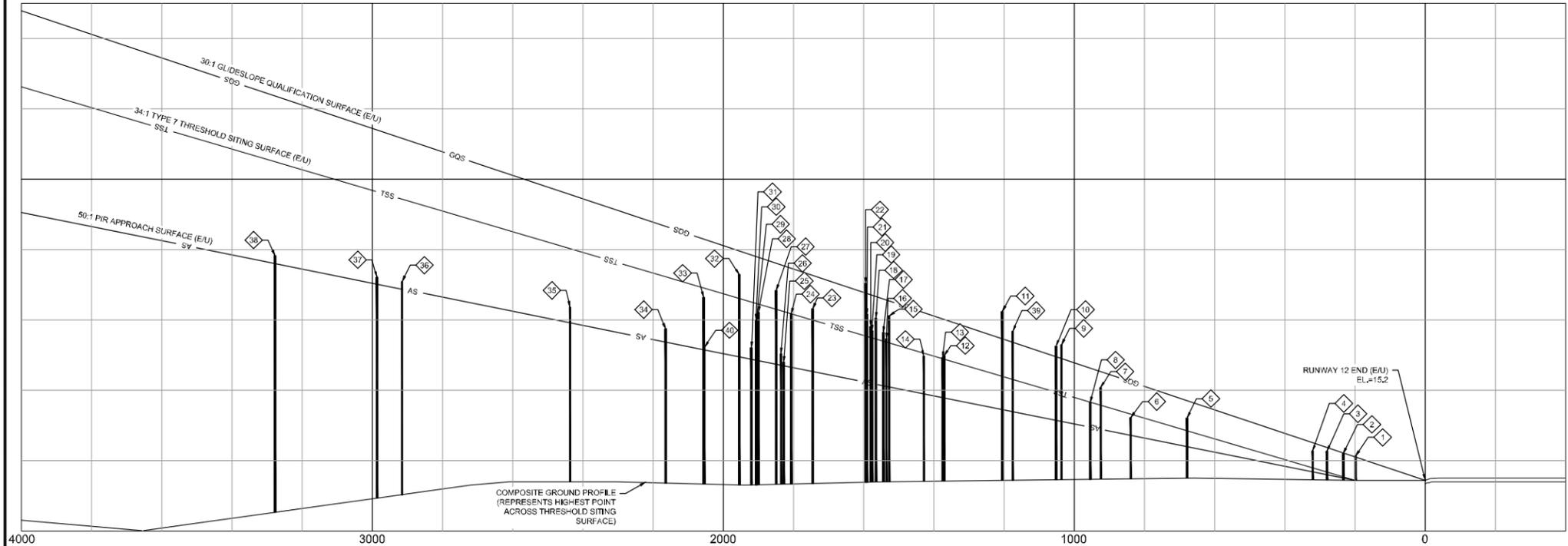
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RUNWAY 12 - PLAN VIEW



- NOTES:
 1. SEE SHEET 9 - INNER APPROACH OBSTRUCTION TABLES FOR RUNWAY 34 INNER APPROACH OBSTRUCTION/DISPOSITION TABLE.
 2. AERIAL PHOTOGRAPH AND OBSTRUCTION SURVEY PROVIDED BY SAM, INC. JULY 2013.

RUNWAY 12 - PROFILE VIEW



LEGEND		
ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 15'	BRL 35'
AIRPORT PROPERTY LINE	E	E(U)
FENCE	X	XX
AIRFIELD PAVEMENT	[Symbol]	[Symbol]
PAVEMENT REMOVAL	[Symbol]	[Symbol]
BEACON	[Symbol]	[Symbol]
FUEL STORAGE AND PUMPS	[Symbol]	SAVE
BUILDINGS - ON AIRPORT	[Symbol]	[Symbol]
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
APPROACH SURFACE	AS	AS(U)
THRESHOLD SITING SURFACE	TSS	TSS(U)
GLIDEPATH QUALIFICATION SURFACE	GQS	GQS(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	[Symbol]	SAVE
GROUND CONTOURS	680	SAVE
PRECISION APPROACH PATH INDICATOR (PAPI)	[Symbol]	[Symbol]
THRESHOLD LIGHTS	[Symbol]	[Symbol]
RUNWAY END IDENTIFIER LIGHTS (RELS)	[Symbol]	[Symbol]
HOLDLINES & SIGNS	[Symbol]	[Symbol]
AIRPORT REFERENCE POINT (ARP)	[Symbol]	[Symbol]
CROP LINE	CRL	SAVE
VEGETATION	[Symbol]	SAVE
FLOW LINE	[Symbol]	SAVE
AVIGATION EASEMENT	[Symbol]	[Symbol]
LIGHT POLE	[Symbol]	SAVE
UTILITY POLE	[Symbol]	SAVE
ELECTRICAL UTILITY LINE	E	SAVE



3010 Gaylord Parkway
 Suite 190
 Frisco, TX 75034
 (972) 377-7480

REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS

AIRPORT LAYOUT PLAN

INNER PORTION
 OF THE
 APPROACH
 SURFACE -
 RUNWAY 12

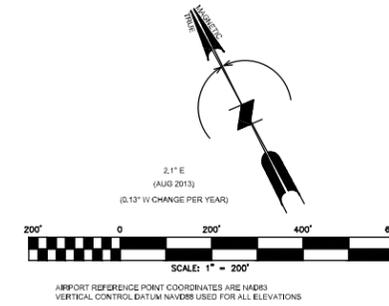
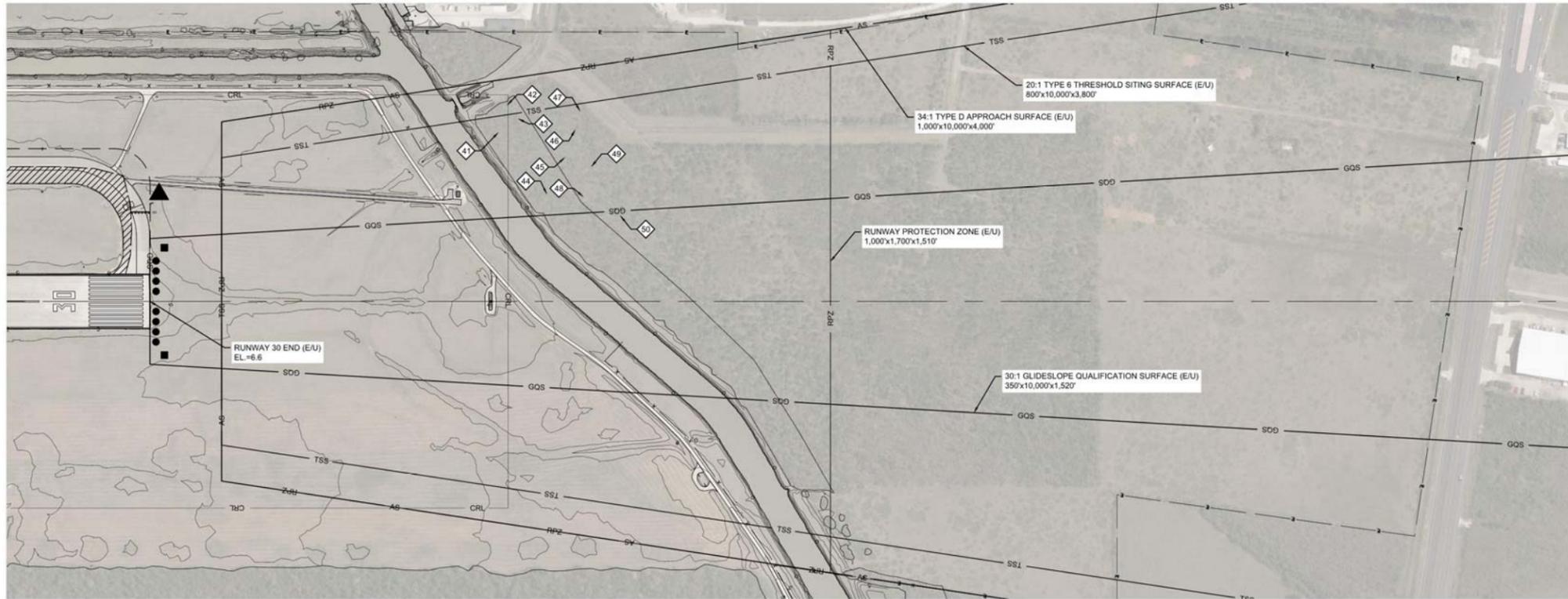
JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

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IPASD-12
 SHEET NUMBER
5

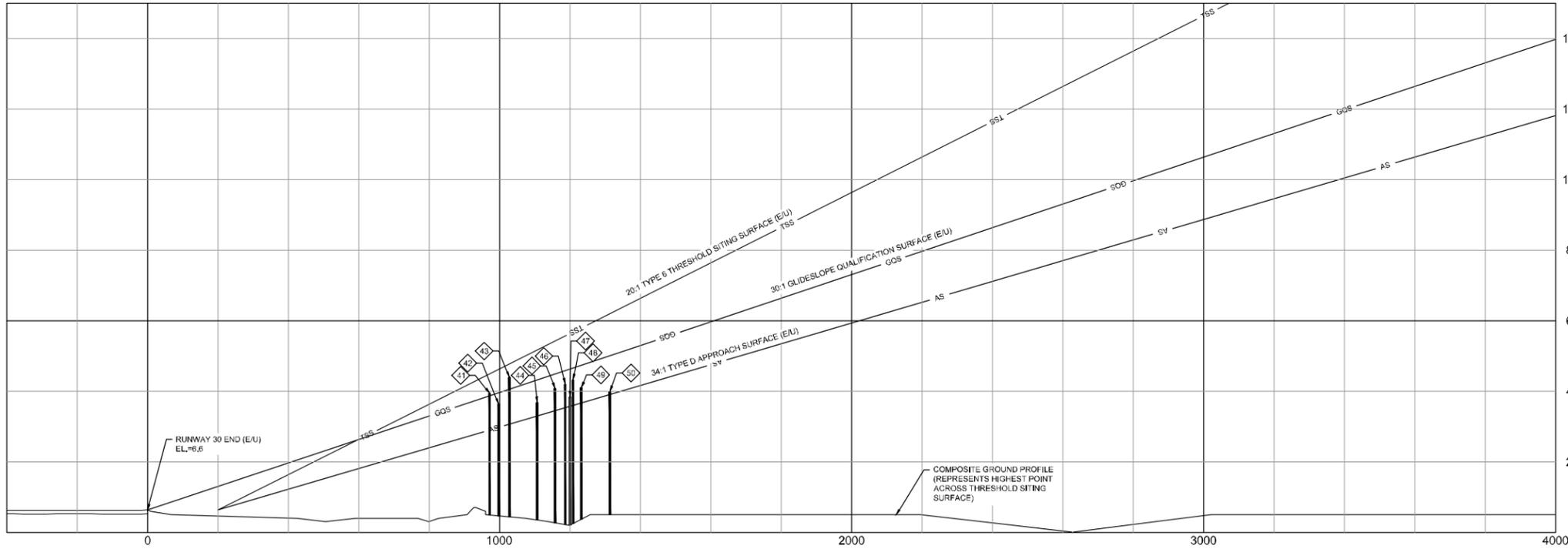
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RUNWAY 30 - PLAN VIEW



- NOTES:
- SEE SHEET 9 - INNER APPROACH OBSTRUCTION TABLES FOR RUNWAY 34 INNER APPROACH OBSTRUCTION/DISPOSITION TABLE.
 - AERIAL PHOTOGRAPH AND OBSTRUCTION SURVEY PROVIDED BY SAM, INC. JULY 2013.

RUNWAY 30 - PROFILE VIEW



LEGEND		
ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 15'	BRL 35'
AIRPORT PROPERTY LINE	E(U)	E(U)
FENCE	X	XX
AIRFIELD PAVEMENT	[Symbol]	[Symbol]
PAVEMENT REMOVAL	[Symbol]	[Symbol]
BEACON	[Symbol]	[Symbol]
FUEL STORAGE AND PUMPS	[Symbol]	SAVE
BUILDINGS - ON AIRPORT	[Symbol]	[Symbol]
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
APPROACH SURFACE	AS	AS(U)
THRESHOLD SITING SURFACE	TSS	TSS(U)
GLIDEPATH QUALIFICATION SURFACE	GQS	GQS(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	[Symbol]	SAVE
GROUND CONTOURS	680	SAVE
PRECISION APPROACH PATH INDICATOR (PAPI)	[Symbol]	[Symbol]
THRESHOLD LIGHTS	[Symbol]	[Symbol]
RUNWAY END IDENTIFIER LIGHTS (RELS)	[Symbol]	[Symbol]
HOLDLINES & SIGNS	[Symbol]	[Symbol]
AIRPORT REFERENCE POINT (ARP)	[Symbol]	[Symbol]
CROP LINE	[Symbol]	CRL
VEGETATION	[Symbol]	SAVE
FLOW LINE	[Symbol]	SAVE
AVIGATION EASEMENT	[Symbol]	[Symbol]
LIGHT POLE	[Symbol]	SAVE
UTILITY POLE	[Symbol]	SAVE
ELECTRICAL UTILITY LINE	E	SAVE



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Suite 190
Frisco, TX 75034
(972) 377-7480

REGISTRATION NO.
F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/POR ARTHUR, TEXAS

AIRPORT LAYOUT PLAN

INNER PORTION
OF THE
APPROACH
SURFACE -
RUNWAY 30

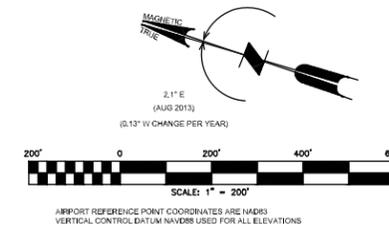
JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

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DRAWING NUMBER
IPASD-30
SHEET NUMBER
6

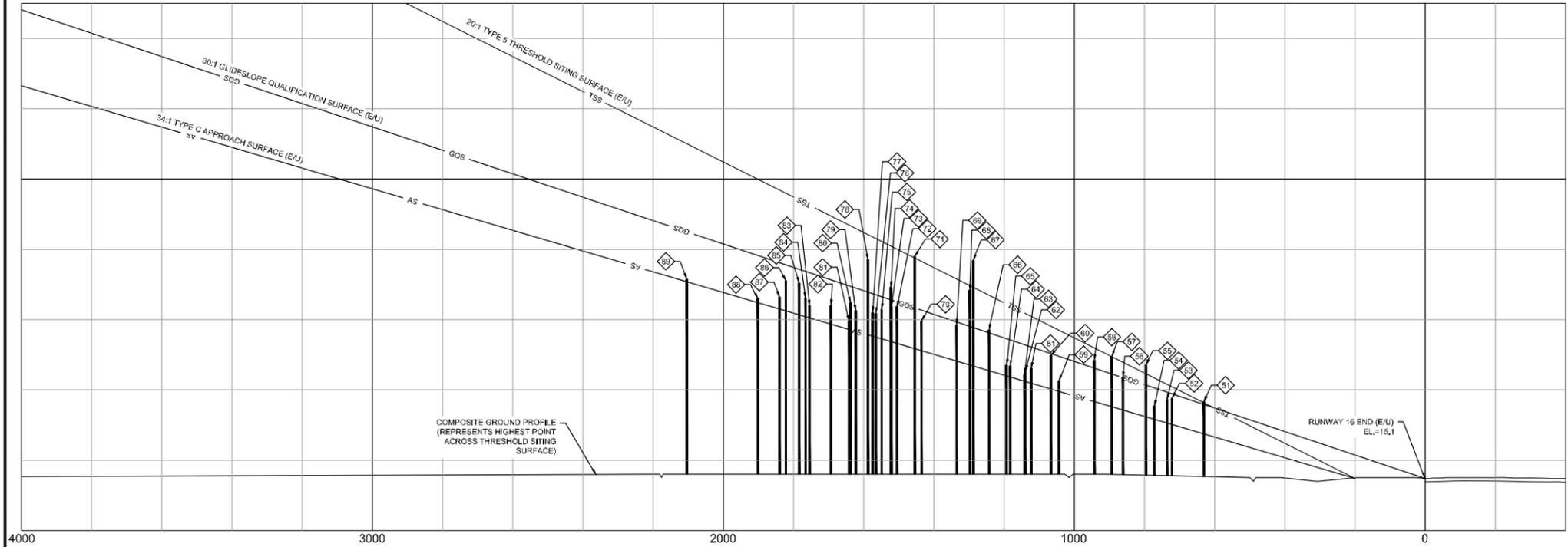
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Last plotted by: Henderson, James, A Plot Style: AECmonocb.ctb Plot Scale: 1:1 Plot Date: 1/5/2015 2:28 PM Plotter used: DWG To PDF.pc3

RUNWAY 16 - PLAN VIEW



- NOTES:
 1. SEE SHEET 9 - INNER APPROACH OBSTRUCTION TABLES FOR RUNWAY 34 INNER APPROACH OBSTRUCTION/DISPOSITION TABLE.
 2. AERIAL PHOTOGRAPH AND OBSTRUCTION SURVEY PROVIDED BY SAM, INC. JULY 2013.

RUNWAY 16 - PROFILE VIEW



LEGEND		
ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 15'	BRL 35'
AIRPORT PROPERTY LINE	E	E(U)
FENCE	X	XX
AIRFIELD PAVEMENT	[Symbol]	[Symbol]
PAVEMENT REMOVAL	[Symbol]	[Symbol]
BEACON	[Symbol]	[Symbol]
FUEL STORAGE AND PUMPS	[Symbol]	SAVE
BUILDINGS - ON AIRPORT	[Symbol]	[Symbol]
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
APPROACH SURFACE	AS	AS(U)
THRESHOLD SITING SURFACE	TSS	TSS(U)
GLIDEPATH QUALIFICATION SURFACE	GOS	GOS(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	[Symbol]	SAVE
GROUND CONTOURS	680	SAVE
PRECISION APPROACH PATH INDICATOR (PAPI)	[Symbol]	[Symbol]
THRESHOLD LIGHTS	[Symbol]	[Symbol]
RUNWAY END IDENTIFIER LIGHTS (RELS)	[Symbol]	[Symbol]
HOLDLINES & SIGNS	[Symbol]	[Symbol]
AIRPORT REFERENCE POINT (ARP)	[Symbol]	[Symbol]
CROP LINE	[Symbol]	CRL
VEGETATION	[Symbol]	SAVE
FLOW LINE	[Symbol]	SAVE
AVIGATION EASEMENT	[Symbol]	[Symbol]
LIGHT POLE	[Symbol]	SAVE
UTILITY POLE	[Symbol]	SAVE
ELECTRICAL UTILITY LINE	E	SAVE



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 (972) 377-7480

REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS
 AIRPORT LAYOUT PLAN

INNER PORTION
 OF THE
 APPROACH
 SURFACE -
 RUNWAY 16

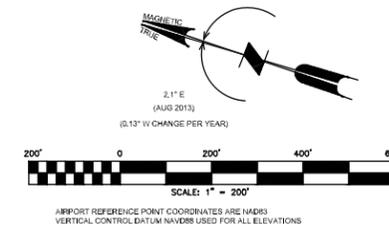
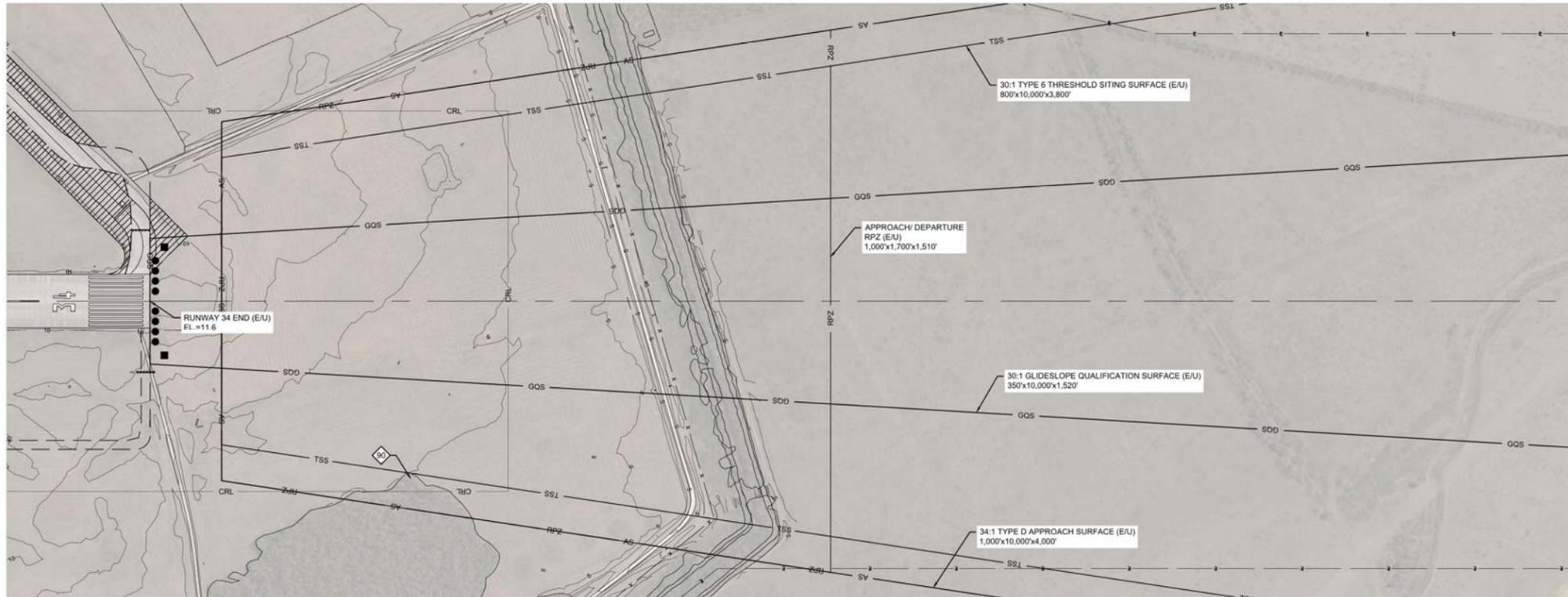
JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

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DRAWING NUMBER
IPASD-16
 SHEET NUMBER
7

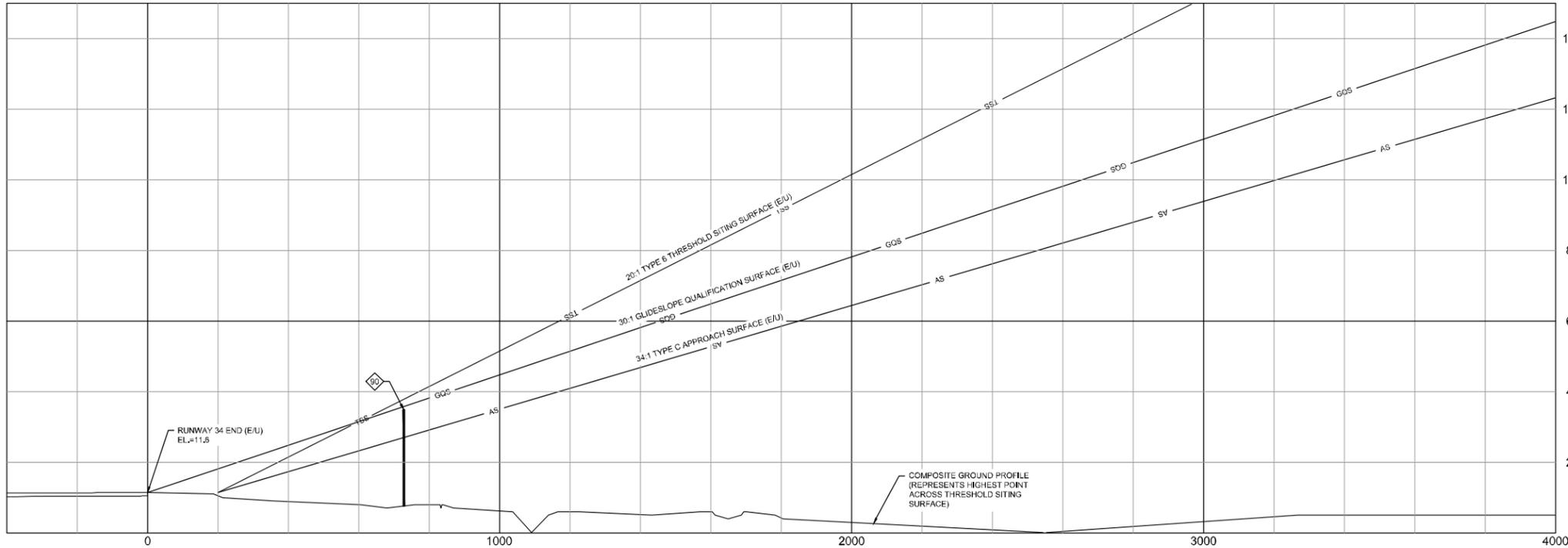
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RUNWAY 34 - PLAN VIEW



- NOTES:
 1. SEE SHEET 9 - INNER APPROACH OBSTRUCTION TABLES FOR RUNWAY 34 INNER APPROACH OBSTRUCTION/DISPOSITION TABLE.
 2. AERIAL PHOTOGRAPH AND OBSTRUCTION SURVEY PROVIDED BY SAM, INC. JULY 2013.

RUNWAY 34 - PROFILE VIEW



LEGEND		
ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 15'	BRL 35'
AIRPORT PROPERTY LINE	E	E(U)
FENCE	X	XX
AIRFIELD PAVEMENT	[Symbol]	[Symbol]
PAVEMENT REMOVAL	[Symbol]	[Symbol]
BEACON	[Symbol]	[Symbol]
FUEL STORAGE AND PUMPS	[Symbol]	SAVE
BUILDINGS - ON AIRPORT	[Symbol]	[Symbol]
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
APPROACH SURFACE	AS	AS(U)
THRESHOLD SITING SURFACE	TSS	TSS(U)
GLIDEPATH QUALIFICATION SURFACE	GOS	GOS(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	[Symbol]	SAVE
GROUND CONTOURS	680	SAVE
PRECISION APPROACH PATH INDICATOR (PAPI)	[Symbol]	[Symbol]
THRESHOLD LIGHTS	[Symbol]	[Symbol]
RUNWAY END IDENTIFIER LIGHTS (RELS)	[Symbol]	[Symbol]
HOLDLINES & SIGNS	[Symbol]	[Symbol]
AIRPORT REFERENCE POINT (ARP)	[Symbol]	[Symbol]
CROP LINE	CRL	CRL
VEGETATION	[Symbol]	SAVE
FLOW LINE	[Symbol]	SAVE
AVIGATION EASEMENT	[Symbol]	[Symbol]
LIGHT POLE	[Symbol]	SAVE
UTILITY POLE	[Symbol]	SAVE
ELECTRICAL UTILITY LINE	E	SAVE



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 (972) 377-7480

REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORTRARTHUR, TEXAS
 AIRPORT LAYOUT PLAN

INNER PORTION
 OF THE
 APPROACH
 SURFACE -
 RUNWAY 34

JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

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IPASD-34
 SHEET NUMBER
8

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REGISTRATION NO.
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REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORTRARTHUR, TEXAS

AIRPORT LAYOUT PLAN

INNER
APPROACH
OBSTRUCTION
TABLES

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

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DRAWING NUMBER
IAOT
SHEET
NUMBER **9**

RUNWAY 12 OBSTRUCTION/DISPOSITION TABLE

DESCRIPTION	TOP ELEVATION	SURFACE PENETRATED	PENETRATION IN FEET	LOCATION	DISPOSITION
1 FENCE	21.3'	AS(E/U)	6.0'	198.1' FM RWY END / 489.0' RIGHT	NONE - CLEAR OF TSS
2 FENCE	22.0'	AS(E/U)	6.0'	233.4' FM RWY END / 464.1' RIGHT	NONE - CLEAR OF TSS
3 FENCE	22.6'	AS(E/U)	6.6'	280.1' FM RWY END / 458.3' RIGHT	NONE - CLEAR OF TSS
4 FENCE	22.7'	AS(E/U)	5.0'	320.3' FM RWY END / 452.8' RIGHT	NONE - CLEAR OF TSS
5 TREE(S)	32.1'	AS(E/U)	8.1'	678.3' FM RWY END / 502.6' LEFT	NONE - CLEAR OF TSS
6 TREE(S)	32.2'	AS(E/U)	5.0'	836.0' FM RWY END / 502.9' LEFT	NONE - CLEAR OF TSS
7 TREE(S)	40.7'	AS(E/U)	11.8'	924.5' FM RWY END / 537.4' RIGHT	NONE - CLEAR OF TSS
8 TREE(S)	36.5'	AS(E/U)	7.0'	954.3' FM RWY END / 505.6' LEFT	NONE - CLEAR OF TSS
9 TREE(S)	52.9'	AS(E/U)	21.8'	1,036' FM RWY END / 632.5' RIGHT	NONE - CLEAR OF TSS
10 TREE(S)	52.5'	TSS(E/U)	13.0'	1,051.8' FM RWY END / 505.5' RIGHT	NONE - CLEAR OF GOS*
11 TREE(S)	62.4'	TSS(E/U)	18.4'	1,204.9' FM RWY END / 510.8' RIGHT	NONE - CLEAR OF GOS*
12 TREE(S)	49.8'	AS(E/U)	12.0'	1,370.0' FM RWY END / 640.36' LEFT	NONE - CLEAR OF TSS
13 TREE(S)	49.3'	AS(E/U)	11.4'	1,374.7' FM RWY END / 620.6' RIGHT	NONE - CLEAR OF TSS
14 TREE(S)	49.7'	AS(E/U)	10.7'	1,4298.4' FM RWY END / 586.2' LEFT	NONE - CLEAR OF TSS
15 TREE(S)	61.1'	TSS(E/U)	7.7'	1,526.9' FM RWY END / 528.0' LEFT	NONE - CLEAR OF GOS*
16 TREE(S)	54.6'	TSS(E/U)	0.9'	1,534.9' FM RWY END / 453.4' LEFT	NONE - CLEAR OF GOS*
17 TREE(S)	56.4'	TSS(E/U)	2.5'	1,543.5' FM RWY END / 574.0' LEFT	NONE - CLEAR OF GOS*
18 TREE(S)	60.5'	TSS(E/U)	5.9'	1,565.4' FM RWY END / 482.1' RIGHT	NONE - CLEAR OF GOS*
19 TREE(S)	56.9'	TSS(E/U)	2.0'	1,576.1' FM RWY END / 397.8' LEFT	NONE - CLEAR OF GOS*
20 TREE(S)	58.1'	TSS(E/U)	3.1'	1,579.3' FM RWY END / 562.0' LEFT	NONE - CLEAR OF GOS*
21 TREE(S)	61.6'	TSS(E/U)	6.3'	1,589.9' FM RWY END / 583.4' RIGHT	NONE - CLEAR OF GOS*
22 TREE(S)	70.4'	AS(E/U)	28.1'	1,594.6' FM RWY END / 633.9' LEFT	NONE - CLEAR OF TSS
23 TREE(S)	63.1'	TSS(E/U)	3.2'	1,745.0' FM RWY END / 287.2' LEFT	NONE - CLEAR OF GOS*
24 TREE(S)	61.7'	AS(E/U)	15.2'	1,808.0' FM RWY END / 339.9' LEFT	NONE - CLEAR OF TSS
25 TREE(S)	47.9'	AS(E/U)	0.9'	1,827.5' FM RWY END / 301.4' LEFT	NONE - CLEAR OF TSS
26 TREE(S)	50.3'	AS(E/U)	3.2'	1,835.1' FM RWY END / 295.4' LEFT	NONE - CLEAR OF TSS
27 TREE(S)	68.4'	TSS(E/U)	5.5'	1,849.1' FM RWY END / 420.3' LEFT	NONE - CLEAR OF GOS*
28 TREE(S)	62.1'	AS(E/U)	13.7'	1,899.8' FM RWY END / 545.1' LEFT	NONE - CLEAR OF TSS
29 TREE(S)	60.0'	AS(E/U)	11.4'	1,904.8' FM RWY END / 291.4' RIGHT	NONE - CLEAR OF TSS
30 TREE(S)	59.9'	AS(E/U)	11.4'	1,906.6' FM RWY END / 301.2' RIGHT	NONE - CLEAR OF TSS
31 TREE(S)	52.1'	AS(E/U)	3.3'	1,919.3' FM RWY END / 270.5' LEFT	NONE - CLEAR OF TSS
32 TREE(S)	72.9'	TSS(E/U)	6.9'	1,954.3' FM RWY END / 384.6' RIGHT	NONE - CLEAR OF GOS*
33 TREE(S)	66.3'	AS(E/U)	14.8'	2,056.0' FM RWY END / 585.4' RIGHT	NONE - CLEAR OF TSS
34 TREE(S)	57.4'	AS(E/U)	3.7'	2,164.1' FM RWY END / 298.2' LEFT	NONE - CLEAR OF TSS
35 TREE(S)	63.6'	AS(E/U)	4.5'	2,436.4' FM RWY END / 237.3' RIGHT	NONE - CLEAR OF TSS
36 TREE(S)	70.9'	AS(E/U)	2.2'	2,915.0' FM RWY END / 387.1' RIGHT	NONE - CLEAR OF TSS
37 TREE(S)	72.1'	AS(E/U)	1.9'	2,986.9' FM RWY END / 281.8' RIGHT	NONE - CLEAR OF TSS
38 TREE(S)	78.2'	AS(E/U)	2.2'	3,277.5' FM RWY END / 98.5' RIGHT	NONE - CLEAR OF TSS
39 TREE(S)	56.7'	AS(E/U)	2.3'	1,174.9' FM RWY END / 623.1' RIGHT	NONE - CLEAR OF TSS
40 TREE(S)	52.2'	AS(E/U)	0.7'	2,053.7' FM RWY END / 191.0' RIGHT	NONE - CLEAR OF TSS

ALL CLEARANCES HAVE BEEN ADJUSTED FOR HEIGHT OF NATURAL OBJECTS, AND 10' FOR PRIVATE ROADS, 15' FOR PUBLIC ROADS, 17' FOR INTERSTATE HIGHWAYS, AND 23' FOR RAILROADS.

*TREES TO BE TOPPED / REMOVED

RUNWAY 30 OBSTRUCTION/DISPOSITION TABLE

DESCRIPTION	TOP ELEVATION	SURFACE PENETRATED	PENETRATION IN FEET	LOCATION	DISPOSITION
1 TREE(S)	39.4'	AS(E/U)	10.4'	971.8' FM RWY END / 469.7' RIGHT	NONE - CLEAR OF TSS
2 TREE(S)	36.3'	AS(E/U)	6.5'	997.4' FM RWY END / 546.9' RIGHT	NONE - CLEAR OF TSS
3 TREE(S)	43.9'	AS(E/U)	13.2'	1,028.3' FM RWY END / 508.3' RIGHT	NONE - CLEAR OF TSS
4 TREE(S)	36.8'	AS(E/U)	3.8'	1,107.5' FM RWY END / 299.2' RIGHT	NONE - CLEAR OF TSS
5 TREE(S)	40.8'	AS(E/U)	6.3'	1,157.8' FM RWY END / 400.6' RIGHT	NONE - CLEAR OF TSS
6 TREE(S)	42.0'	AS(E/U)	6.7'	1,186.4' FM RWY END / 476.8' RIGHT	NONE - CLEAR OF TSS
7 TREE(S)	38.3'	AS(E/U)	2.8'	1,200.5' FM RWY END / 530.4' RIGHT	NONE - CLEAR OF TSS
8 TREE(S)	43.2'	AS(E/U)	7.2'	1,209.0' FM RWY END / 291.9' RIGHT	NONE - CLEAR OF TSS
9 TREE(S)	41.0'	AS(E/U)	4.3'	1,232.8' FM RWY END / 375.7' RIGHT	NONE - CLEAR OF TSS
10 TREE(S)	39.9'	AS(E/U)	0.8'	1,313.5' FM RWY END / 238.0' RIGHT	NONE - CLEAR OF TSS

ALL CLEARANCES HAVE BEEN ADJUSTED FOR HEIGHT OF NATURAL OBJECTS, AND 10' FOR PRIVATE ROADS, 15' FOR PUBLIC ROADS, 17' FOR INTERSTATE HIGHWAYS, AND 23' FOR RAILROADS.

*TREES TO BE TOPPED / REMOVED

RUNWAY 16 OBSTRUCTION/DISPOSITION TABLE

DESCRIPTION	TOP ELEVATION	SURFACE PENETRATED	PENETRATION IN FEET	LOCATION	DISPOSITION
1 POLE	36.6'	AS(E/U)	9.2'	630.6' FM RWY END / 561.8' LEFT	NONE - CLEAR OF TSS
2 POLE	37.6'	AS(E/U)	7.5'	721.5' FM RWY END / 531.9' LEFT	NONE - CLEAR OF TSS
3 POLE	37.2'	AS(E/U)	6.7'	735.1' FM RWY END / 456.3' LEFT	NONE - CLEAR OF TSS
4 BUILDING	35.4'	AS(E/U)	3.8'	771.7' FM RWY END / 384.8' LEFT	NONE - CLEAR OF TSS
5 TREE(S)	47.0'	TSS(E/U)	2.4'	795.7' FM RWY END / 443.6' LEFT	NONE - CLEAR OF GOS*
6 TREE(S)	43.5'	AS(E/U)	9.3'	860.6' FM RWY END / 454.9' LEFT	NONE - CLEAR OF TSS
7 POLE	49.4'	AS(E/U)	14.2'	892.7' FM RWY END / 527.2' LEFT	NONE - CLEAR OF TSS
8 TREE(S)	48.4'	AS(E/U)	11.8'	942.6' FM RWY END / 588.2' RIGHT	NONE - CLEAR OF TSS
9 POLE	42.5'	AS(E/U)	2.9'	1,043.4' FM RWY END / 543.0' LEFT	NONE - CLEAR OF TSS
10 TREE(S)	49.8'	AS(E/U)	9.5'	1,066.5' FM RWY END / 532.1' RIGHT	NONE - CLEAR OF TSS
11 TREE(S)	46.1'	AS(E/U)	4.2'	1,122.9' FM RWY END / 576.1' RIGHT	NONE - CLEAR OF TSS
12 TREE(S)	45.4'	AS(E/U)	3.0'	1,138.9' FM RWY END / 476.8' LEFT	NONE - CLEAR OF TSS
13 TREE(S)	44.4'	AS(E/U)	1.9'	1,141.0' FM RWY END / 389.9' LEFT	NONE - CLEAR OF TSS
14 TREE(S)	46.7'	AS(E/U)	3.0'	1,182.8' FM RWY END / 610.8' RIGHT	NONE - CLEAR OF TSS
15 POLE	47.0'	AS(E/U)	3.0'	1,192.5' FM RWY END / 557.6' LEFT	NONE - CLEAR OF TSS
16 TREE(S)	57.0'	AS(E/U)	11.6'	1,242.6' FM RWY END / 561.4' RIGHT	NONE - CLEAR OF TSS
17 TREE(S)	76.8'	AS(E/U)	30.0'	1,287.5' FM RWY END / 644.9' LEFT	NONE - CLEAR OF TSS
18 TREE(S)	68.3'	AS(E/U)	21.3'	1,297.0' FM RWY END / 561.4' RIGHT	NONE - CLEAR OF TSS
19 TREE(S)	58.4'	AS(E/U)	10.2'	1,335.1' FM RWY END / 483.9' LEFT	NONE - CLEAR OF TSS
20 TREE(S)	59.6'	AS(E/U)	8.5'	1,435.1' FM RWY END / 588.6' RIGHT	NONE - CLEAR OF TSS
21 TREE(S)	77.9'	AS(E/U)	26.2'	1,454.0' FM RWY END / 602.4' LEFT	NONE - CLEAR OF TSS
22 TREE(S)	63.7'	AS(E/U)	10.5'	1,504.6' FM RWY END / 625.0' RIGHT	NONE - CLEAR OF TSS
23 TREE(S)	63.8'	AS(E/U)	10.2'	1,521.7' FM RWY END / 666.3' LEFT	NONE - CLEAR OF TSS
24 TREE(S)	69.1'	AS(E/U)	15.4'	1,522.1' FM RWY END / 429.4' LEFT	NONE - CLEAR OF TSS
25 TREE(S)	62.9'	AS(E/U)	8.5'	1,548.4' FM RWY END / 572.1' LEFT	NONE - CLEAR OF TSS
26 TREE(S)	61.7'	AS(E/U)	6.8'	1,564.7' FM RWY END / 642.8' RIGHT	NONE - CLEAR OF TSS
27 TREE(S)	62.0'	AS(E/U)	6.8'	1,574.7' FM RWY END / 630.3' RIGHT	NONE - CLEAR OF TSS
28 TREE(S)	77.0'	AS(E/U)	21.4'	1,588.0' FM RWY END / 473.6' RIGHT	NONE - CLEAR OF TSS
29 TREE(S)	62.5'	AS(E/U)	5.9'	1,622.3' FM RWY END / 539.4' RIGHT	NONE - CLEAR OF TSS
30 TREE(S)	64.7'	AS(E/U)	7.7'	1,637.0' FM RWY END / 440.7' RIGHT	NONE - CLEAR OF TSS
31 TREE(S)	59.5'	AS(E/U)	2.3'	1,641.4' FM RWY END / 572.2' RIGHT	NONE - CLEAR OF TSS
32 TREE(S)	64.0'	AS(E/U)	5.3'	1,693.0' FM RWY END / 485.9' LEFT	NONE - CLEAR OF TSS
33 TREE(S)	63.9'	AS(E/U)	3.4'	1,754.6' FM RWY END / 691.5' LEFT	NONE - CLEAR OF TSS
34 TREE(S)	66.1'	AS(E/U)	5.3'	1,765.8' FM RWY END / 540.2' RIGHT	NONE - CLEAR OF TSS
35 TREE(S)	70.3'	AS(E/U)	9.0'	1,783.3' FM RWY END / 518.9' LEFT	NONE - CLEAR OF TSS
36 TREE(S)	71.0'	AS(E/U)	8.5'	1,821.7' FM RWY END / 427.4' LEFT	NONE - CLEAR OF TSS
37 TREE(S)	66.4'	AS(E/U)	3.4'	1,839.2' FM RWY END / 306.3' LEFT	NONE - CLEAR OF TSS
38 TREE(S)	65.9'	AS(E/U)	1.1'	1,900.8' FM RWY END / 735.4' LEFT	NONE - CLEAR OF TSS
39 TREE(S)	71.4'	AS(E/U)	0.6'	2,103.3' FM RWY END / 417.2' RIGHT	NONE - CLEAR OF TSS

ALL CLEARANCES HAVE BEEN ADJUSTED FOR HEIGHT OF NATURAL OBJECTS, AND 10' FOR PRIVATE ROADS, 15' FOR PUBLIC ROADS, 17' FOR INTERSTATE HIGHWAYS, AND 23' FOR RAILROADS.

*TREES TO BE TOPPED / REMOVED

RUNWAY 34 OBSTRUCTION/DISPOSITION TABLE

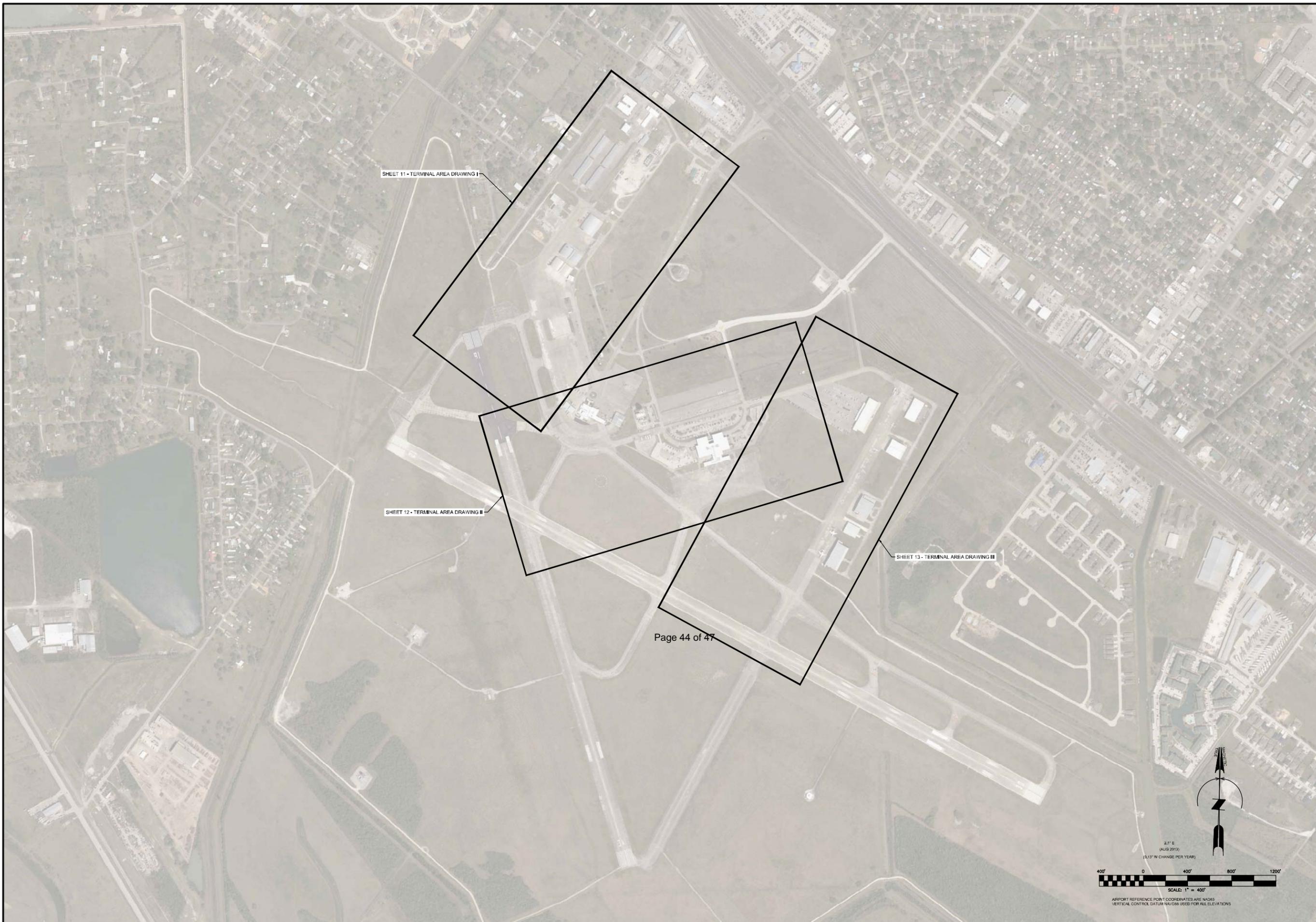
DESCRIPTION	TOP ELEVATION	SURFACE PENETRATED	PENETRATION IN FEET	LOCATION	DISPOSITION
1 TREE(S)	34.9'	AS(E/U)	7.9'	728.0' FM RWY END / 494.8' LEFT	NONE - CLEAR OF TSS

ALL CLEARANCES HAVE BEEN ADJUSTED FOR HEIGHT OF NATURAL OBJECTS, AND 10' FOR PRIVATE ROADS, 15' FOR PUBLIC ROADS, 17' FOR INTERSTATE HIGHWAYS, AND 23' FOR RAILROADS.

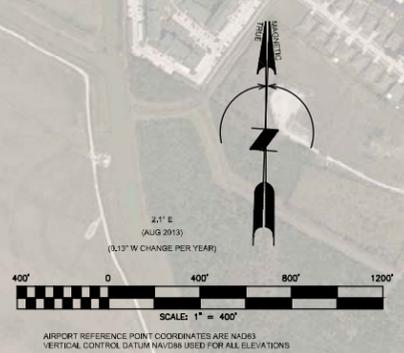
*TREES TO BE TOPPED / REMOVED

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 Last plotted by: Henderson, James, A Plot Style: AECmonocb Plot Scale: 1:1 Plot Date: 1/5/2015 2:30 PM Plotter used: DWG To PDF.pc3



Page 44 of 47



3010 Gaylord Parkway
 Suite 190
 Frisco, TX 75034
 (972) 377-7480

REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS

AIRPORT LAYOUT PLAN

TERMINAL AREA
 KEY MAP

JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

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DRAWING NUMBER

TAKM

SHEET NUMBER **10**



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REGISTRATION NO.
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REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORTR ARTHUR, TEXAS

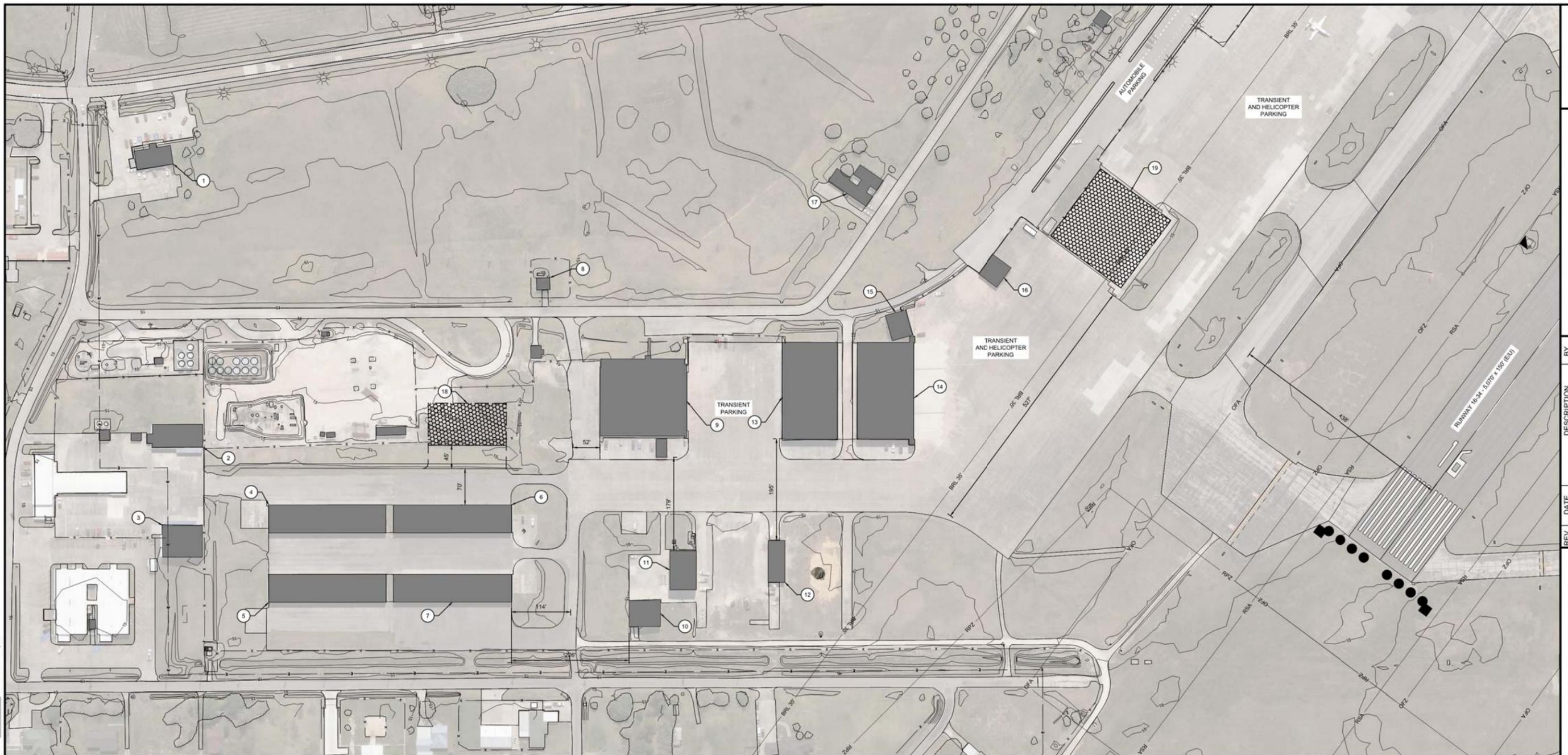
AIRPORT LAYOUT PLAN

TERMINAL AREA
DRAWING I

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

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DRAWING NUMBER
TAD-1
SHEET NUMBER
11



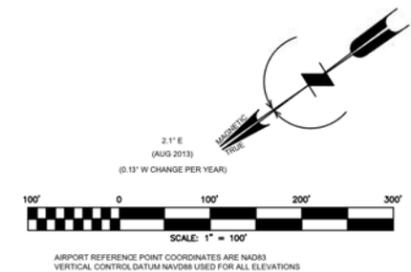
LEGEND

ITEM	INITIAL	ULTIMATE
BUILDING RESTRICTION LINE	BRL 35'	BRL 35'
AIRPORT PROPERTY LINE	E	E(U)
FENCE	X	XX
AIRFIELD PAVEMENT	[Symbol]	[Symbol]
PAVEMENT REMOVAL	[Symbol]	[Symbol]
BEACON	[Symbol]	[Symbol]
FUEL STORAGE AND PUMPS	[Symbol]	[Symbol]
BUILDINGS - ON AIRPORT	[Symbol]	[Symbol]
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	[Symbol]	[Symbol]
HOLDLINES & SIGNS	[Symbol]	[Symbol]
AIRPORT REFERENCE POINT (ARP)	[Symbol]	[Symbol]

EXISTING BUILDING TABLE

NO.	STRUCTURE	TOP ELEVATION
1	JEFFERSON COUNTY TAX OFFICE	26.00'
2	MOSQUITO CONTROL HANGAR	31.13'
3	MOSQUITO CONTROL HANGAR	34.22'
4	T-HANGAR	28.69'
5	T-HANGAR	28.58'
6	T-HANGAR	28.77'
7	T-HANGAR	29.15'
8	RENTAL CAR PREP	22.91'
9	HANGAR #4	29.76'
10	AIRCRAFT EQUIPMENT STORAGE BUILDING	31.39'
11	MAINTENANCE BUILDING	39.10'
12	MAINTENANCE POLE BARN	26.89'
13	HANGAR #3	36.33'
14	HANGAR #2	36.09'
15	AIRPORT COMPUTER SHOP	31.55'
16	OFFICE BUILDING	27.91'
17	AIRPORT MANAGEMENT	25.15'
18	150' x 80' HANGAR	TBD
19	160' x 160' HANGAR	TBD

ALL BUILDINGS ARE CLEAR OF FAR PART 77 AIRSPACE SURFACES.



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JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS

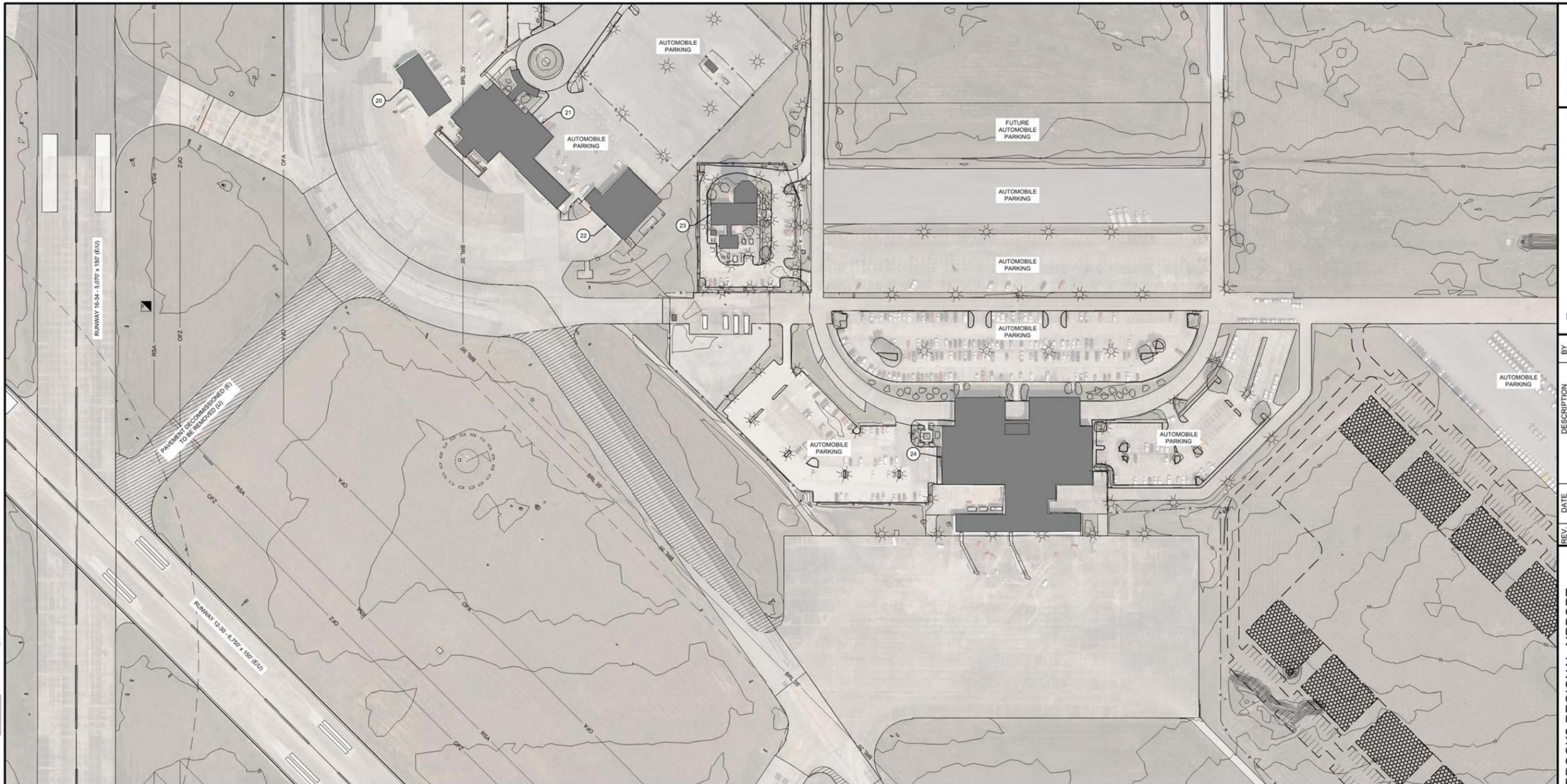
AIRPORT LAYOUT PLAN

TERMINAL AREA
DRAWING II

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

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DRAWING NUMBER
TAD-2
SHEET NUMBER
12



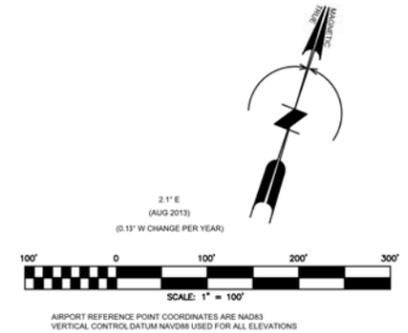
LEGEND

ITEM	INITIAL	ULTIMATE
BUILDING RESTRICTION LINE	BRL 35'	BRL 35'
AIRPORT PROPERTY LINE	E	E(U)
FENCE	X	XX
AIRFIELD PAVEMENT	[Symbol]	[Symbol]
PAVEMENT REMOVAL	[Symbol]	[Symbol]
BEACON	[Symbol]	[Symbol]
FUEL STORAGE AND PUMPS	[Symbol]	SAME
BUILDINGS - ON AIRPORT	[Symbol]	[Symbol]
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	[Symbol]	[Symbol]
HOLDLINES & SIGNS	[Symbol]	[Symbol]
AIRPORT REFERENCE POINT (ARP)	[Symbol]	[Symbol]

EXISTING BUILDING TABLE

NO.	STRUCTURE	TOP ELEVATION
20	ARFF STATION	35.13'
21	EXECUTIVE TERMINAL	52.01'
22	HANGAR #6	34.46'
23	AIR TRAFFIC CONTROL TOWER	108.13'
24	AIR CARRIER PASSENGER TERMINAL	35.76'

ALL BUILDINGS ARE CLEAR OF FAR PART 77 AIRSPACE SURFACES.



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REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORTRARTHUR, TEXAS

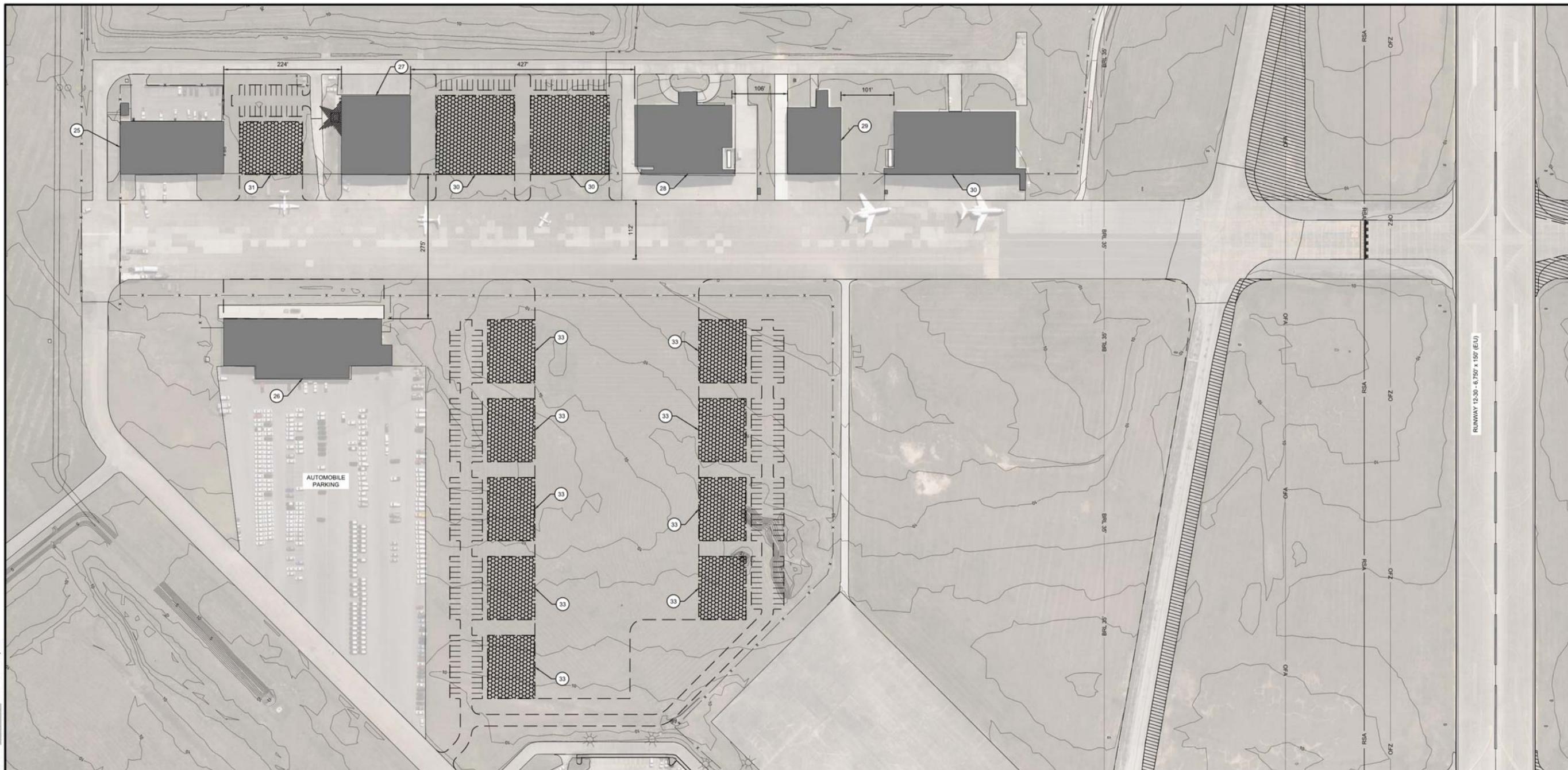
AIRPORT LAYOUT PLAN

TERMINAL AREA
DRAWING III

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

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DRAWING NUMBER
TAD-3
SHEET NUMBER
13



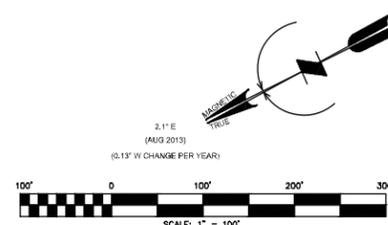
LEGEND

ITEM	INITIAL	ULTIMATE
BUILDING RESTRICTION LINE	BRL 35'	BRL 35'
AIRPORT PROPERTY LINE	P	P(U)
FENCE	X	XX
AIRFIELD PAVEMENT	[Symbol]	[Symbol]
PAVEMENT REMOVAL	[Symbol]	[Symbol]
BEACON	[Symbol]	[Symbol]
FUEL STORAGE AND PUMPS	[Symbol]	SAME
BUILDINGS - ON AIRPORT	[Symbol]	[Symbol]
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	[Symbol]	[Symbol]
HOLDLINES & SIGNS	[Symbol]	[Symbol]
AIRPORT REFERENCE POINT (ARP)	[Symbol]	[Symbol]

EXISTING BUILDING TABLE

NO.	STRUCTURE	TOP ELEVATION
(25)	COUNTY SHERIFF'S DEPARTMENT	25.56'
(26)	HANGAR 7	30.68'
(27)	HANGAR 5	18.65'
(28)	PRIVATE - LAND LEASE HANGAR	19.75'
(29)	PRIVATE - LAND LEASE HANGAR	29.78'
(30)	PRIVATE - LAND LEASE HANGAR	42.88'
(31)	120' x 100' CORPORATE HANGAR	TBD
(32)	150' x 150' CORPORATE HANGAR	TBD
(33)	120' x 90' CORPORATE HANGAR	TBD

ALL BUILDINGS ARE CLEAR OF FAR PART 77 AIRSPACE SURFACES.



AIRPORT REFERENCE POINT COORDINATES ARE NAD83
VERTICAL CONTROL DATUM NAVD83 USED FOR ALL ELEVATIONS

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REGISTRATION NO.
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REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS

AIRPORT LAYOUT PLAN

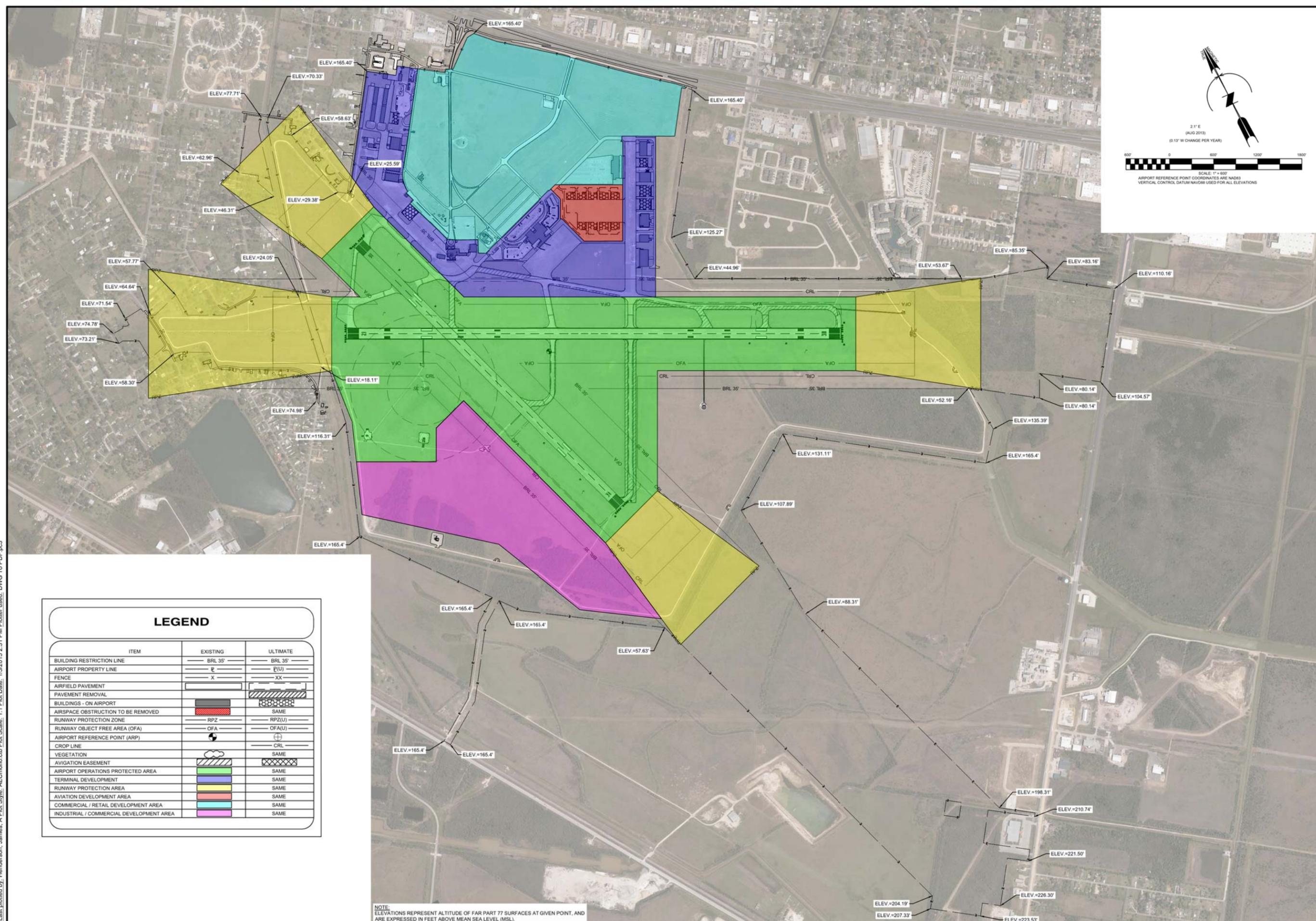
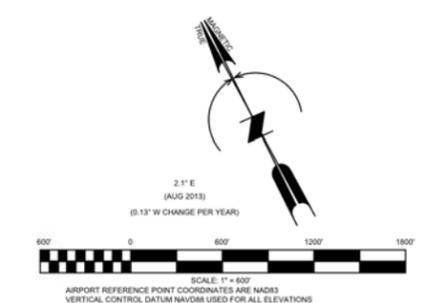
LAND USE
DRAWING

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

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DRAWING NUMBER
LUD

SHEET NUMBER
14



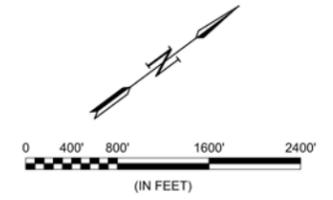
LEGEND

ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 35'	BRL 35'
AIRPORT PROPERTY LINE	⊔	⊔(U)
FENCE	X	XX
AIRFIELD PAVEMENT	▬	▬
PAVEMENT REMOVAL	▬	▬
BUILDINGS - ON AIRPORT	■	■
AIRSPACE OBSTRUCTION TO BE REMOVED	■	SAME
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
RUNWAY OBJECT FREE AREA (OFA)	OFA	OFA(U)
AIRPORT REFERENCE POINT (ARP)	⊕	⊕
CROP LINE	⊕	⊕
VEGETATION	☁	SAME
AVIGATION EASEMENT	▬	▬
AIRPORT OPERATIONS PROTECTED AREA	▬	SAME
TERMINAL DEVELOPMENT	▬	SAME
RUNWAY PROTECTION AREA	▬	SAME
AVIATION DEVELOPMENT AREA	▬	SAME
COMMERCIAL / RETAIL DEVELOPMENT AREA	▬	SAME
INDUSTRIAL / COMMERCIAL DEVELOPMENT AREA	▬	SAME

NOTE:
ELEVATIONS REPRESENT ALTITUDE OF FAR PART 77 SURFACES AT GIVEN POINT, AND ARE EXPRESSED IN FEET ABOVE MEAN SEA LEVEL (MSL).

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REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS

AIRPORT LAYOUT PLAN

AIRPORT
 PROPERTY KEY
 MAP

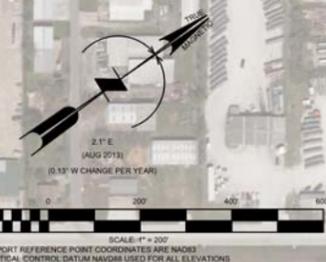
JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

BAR IS ONE INCH ON
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 SCALES ACCORDINGLY.

DRAWING NUMBER
APKM

SHEET
 NUMBER **15**

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NOTE:
 FOR PROPERTY OWNERSHIP INFORMATION SEE
 SHEET 23 - AIRPORT PROPERTY TABLES

REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY

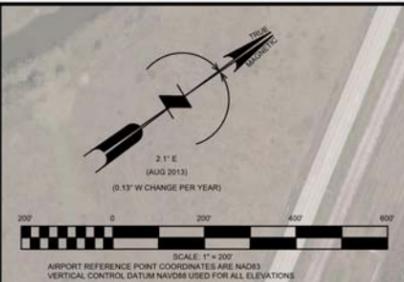
JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS
 AIRPORT LAYOUT PLAN

AIRPORT
 PROPERTY MAP
 I

JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

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 THIS SHEET, ADJUST
 SCALES ACCORDINGLY.

DRAWING NUMBER
APM-1
 SHEET
NUMBER 16



NOTE:
FOR PROPERTY OWNERSHIP INFORMATION SEE
SHEET 23 - AIRPORT PROPERTY TABLES

MATCHLINE SHEET 16 - AIRPORT PROPERTY MAP I



3010 Gaylord Parkway
Suite 190
Frisco, TX 75034
(972) 377-7480

REGISTRATION NO.
F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS
AIRPORT LAYOUT PLAN

AIRPORT
PROPERTY MAP
II

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

BAR IS ONE INCH ON
ORIGINAL DRAWING
IF NOT ONE INCH ON
THIS SHEET, ADJUST
SCALES ACCORDINGLY.

DRAWING NUMBER
APM-2

SHEET
NUMBER 17

File: L:\2012\12121740 - BPT ALP Update & Obstruction Survey\Drawings\BPT-APM-14.dwg Last Save: 1/5/2015 1:27 PM Last saved by: Pihavener
Last plotted by: Henderson, James, A Plot Style: AECmono.ctb Plot Scale: 1:1 Plot Date: 1/5/2015 2:33 PM Plotter used: DWG To PDF.pc3

File: L:\2012\12121740 - BPT ALP Update & Obstruction Survey\Drawings\BPT-APM-14.dwg Last Save: 1/5/2015 1:27 PM Last saved by: Pihavener
 Last plotted by: Henderson, James, A Plot Style: AECmonochrome.ctb Plot Scale: 1:1 Plot Date: 1/5/2015 2:33 PM Plotter used: DWG To PDF.pc3



SCALE: 1" = 200'
 AIRPORT REFERENCE POINT COORDINATES ARE NAD83
 VERTICAL CONTROL DATUM NAVD83 USED FOR ALL ELEVATIONS

NOTE:
 FOR PROPERTY OWNERSHIP INFORMATION SEE SHEET 23 - AIRPORT PROPERTY TABLES



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REGISTRATION NO.
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REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS

AIRPORT LAYOUT PLAN

AIRPORT
 PROPERTY MAP
 III

JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

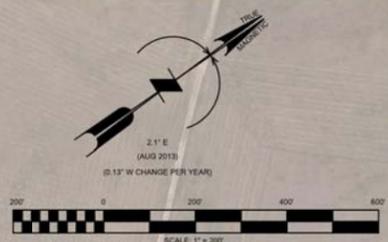
BAR IS ONE INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER
APM-3
 SHEET NUMBER
18

File: L:\2012\12121740 - BPT ALP Update & Obstruction Survey\Drawings\BPT-APM-14.dwg Last Save: 1/5/2015 1:27 PM Last saved by: Pihavener
 Last Plotted by: Henderson, James, A Plot Style: AECmono.ctb Plot Scale: 1:1 Plot Date: 1/5/2015 2:34 PM Plotter used: DWG To PDF.pc3



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NOTE:
 FOR PROPERTY OWNERSHIP INFORMATION SEE
 SHEET 23 - AIRPORT PROPERTY TABLES

REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS
 AIRPORT LAYOUT PLAN

AIRPORT
 PROPERTY MAP
 IV

JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

BAR IS ONE INCH ON
 ORIGINAL DRAWING
 1" = 400'
 IF NOT ONE INCH ON
 THIS SHEET, ADJUST
 SCALES ACCORDINGLY.

DRAWING NUMBER
APM-4

SHEET
 NUMBER **19**

File: L:\2012\12121740 - BPT ALP Update & Obstruction Survey\Drawings\BPT-APM-14.dwg Last Save: 1/5/2015 1:27 PM Last saved by: Plhavener
 Last plotted by: Henderson, James_A Plot Style: AECmono.ctb Plot Scale: 1:1 Plot Date: 1/5/2015 2:34 PM Plotter used: DWG To PDF.pc3



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REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY

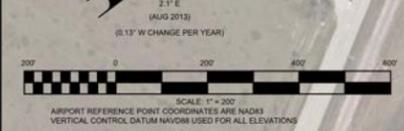
JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS
 AIRPORT LAYOUT PLAN

AIRPORT
 PROPERTY MAP
 V

JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

BAR IS ONE INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER
APM-5
 SHEET NUMBER
20



NOTE:
FOR PROPERTY OWNERSHIP INFORMATION SEE
SHEET 23 - AIRPORT PROPERTY TABLES



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Frisco, TX 75034
(972) 377-7480

REGISTRATION NO.
F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS

AIRPORT LAYOUT PLAN

AIRPORT
PROPERTY MAP
VI

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

BAR IS ONE INCH ON
ORIGINAL DRAWING
IF NOT ONE INCH ON
THIS SHEET, ADJUST
SCALES ACCORDINGLY.

DRAWING NUMBER
APM-6

SHEET
NUMBER **21**

File: L:\2012\12121740 - BPT ALP Update & Obstruction Survey\Drawings\BPT-APM-14.dwg Last Save: 1/5/2015 1:27 PM Last saved by: Pihavener
Last plotted by: Henderson, James_A Plot Style: AECmono.ctb Plot Scale: 1:1 Plot Date: 1/5/2015 2:34 PM Plotter used: DWG To PDF.pc3

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 Last plotted by: Henderson, James, A Plot Style: AECmonocb.ctb Plot Scale: 1:1 Plot Date: 1/5/2015 2:35 PM Plotter used: DWG To PDF.pc3

OWNERSHIP TABLE			
TRACT	GRANTOR	DATE	DESCRIPTION
1	FRANK E. RANDALL AND WIFE, MAY C. RANDALL	11/25/1957	VOL. 1091, PAGE 587 LOT 47 SHAMROCK ACRES
2	ROY N. WOOD AND JOHNNIE WOOD	10/14/1957	VOL. 1087, PAGE 137 LOT 48 SHAMROCK ACRES
3	J. H. KILLBUCK AND WIFE, IRMA LEOMA KILLBUCK	10/14/1957	VOL. 1087, PAGE 149 LOT 44 SHAMROCK ACRES
4	DLBERT C. WHITEHEAD AND WIFE, MACLE WHITEHEAD	3/5/1957	VOL. 1104, PAGE 550 LOT 43 SHAMROCK ACRES
5	EARL J. WIEGMANN AND ELIZABETH KEITH WIEGMANN	11/4/1957	VOL. 1088, PAGE 270 LOT 42 SHAMROCK ACRES
6	HURSHEL A. HOYE AND EVELYN L. HOYE	6/5/1958	VOL. 1117, PAGE 459 PART OF LOT 41 SHAMROCK ACRES
7	CARL R. BRADLEY AND MARIE BRADLEY	11/20/1957	VOL. 1091, PAGE 605 PART OF LOT 39 SHAMROCK ACRES
8	MARY E. YOUNG, etal	8/16/1955	VOL. 993, PAGE 65 TRACT A-3, 1.08 ACRES
9	MARY E. YOUNG, etal	8/16/1955	VOL. 993, PAGE 65 TRACT A-4, 0.032 ACRES
10	MARY E. YOUNG, etal	8/16/1955	VOL. 993, PAGE 65 SEVERAGE TRACT 51.197 ACRES
11	MARY E. YOUNG, etal	8/16/1955	VOL. 993, PAGE 65 TRACT A-1, 37.22 ACRES
12	MARY E. YOUNG, etal	8/16/1955	VOL. 993, PAGE 65 TRACT A-2, 52.08 ACRES
13	ROY C. WILSON AND WIFE, RUTH ANN WILSON	8/8/1955	VOL. 990, PAGE 73 TRACT G-1, 2.485 ACRES
14	ROY C. WILSON AND WIFE, RUTH ANN WILSON	8/8/1955	VOL. 990, PAGE 73 TRACT G-2, 0.248 ACRES
15	MARY E. YOUNG, etal	8/16/1955	VOL. 993, PAGE 65 TRACT C-1, 0.974 ACRES
16	WILLIAM BOSTICK, etux	12/3/1970	VOL. 1063, PAGE 56 PART OF LOTS 16 & 17, BLOCK 7, HILLCREST SECOND ADDITION
17	R. D. DAWSON	4/16/1956	VOL. 1023, PAGE 378 LOT 18, BLOCK 7, HILLCREST SECOND ADDITION
18	RICHARD E. DOORNBOSS, etal	3/7/2001	CF#2001008588 PARCEL 5B 4.286 ACRES
19	C. DOORNBOSS	7/22/1941	VOL. 499, PAGE 136 144.86 ACRES, FIRST TRACT
20	W. W. KYLE	2/10/1941	VOL. 499, PAGE 375 382.86 ACRES
21	MARY E. YOUNG, etal	8/16/1955	VOL. 993, PAGE 65 TRACT C-2, 0.825 ACRES
22	ESTATE OF P. H. WIESS	8/20/1941	VOL. 499, PAGE 376 102.48 ACRES
23	MCFADDIN TRUST	2/9/1942	VOL. 505, PAGE 167 8.56 ACRES
24	MARY E. YOUNG, etal	7/21/1941	VOL. 499, PAGE 138 31.57 ACRES, FIRST TRACT
25	J. D. FORD AND WIFE, VERA FORD	4/27/1957	VOL. 1068, PAGE 582 17.183 ACRES
26	J. D. FORD AND WIFE, VERA FORD	4/27/1957	VOL. 1068, PAGE 582 11.321 ACRES
27	EMMETT E. DARNELL AND WIFE, KATHERINE M. DARNELL		VOL. 1208, PAGE 296 0.50 ACRES
28	CHARLES H. HOOKER AND BOBBIE HOOKER	12/26/1957	VOL. 1095, PAGE 139 PARCEL A
29	CHARLES H. HOOKER AND BOBBIE HOOKER	12/26/1957	VOL. 1095, PAGE 139 PARCEL B
30	C. DOORNBOSS	7/22/1941	VOL. 499, PAGE 136 185.12 ACRES, SECOND TRACT
31	J. D. FORD AND WIFE, VERA FORD	6/15/1955	VOL. 1008, PAGE 273 TRACT 1, 3.68 ACRES
32	J. D. FORD AND WIFE, VERA FORD	6/15/1955	VOL. 1008, PAGE 273 TRACT 2, 0.016 ACRES
33	ESTATE OF P. H. WIESS	2/14/1956	VOL. 1018, PAGE 195 TRACT 14, 14.70 ACRES
34	ESTATE OF P. H. WIESS	2/14/1956	VOL. 1018, PAGE 195 TRACT 14A, 0.90 ACRES
35	MARY E. YOUNG, etal	7/21/1941	VOL. 499, PAGE 138 24.99 ACRES, SECOND TRACT
36	VERA G. MCVFAN	7/21/1941	VOL. 1030, PAGE 80 PART OF LOTS 12 & 13, BLOCK 8, HILLCREST PLACE ADDITION
37	J. D. FORD AND WIFE, VERA FORD	4/27/1957	VOL. 1068, PAGE 582 0.126 ACRES
38	HARRY J. COMSTOCK AND WIFE, DELORES J. COMSTOCK	8/20/1969	VOL. 1608, PAGE 385 PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION
39	GILBERT L. BUZZINGHAM	9/15/1969	VOL. 1610, PAGE 361 PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION
40	FREDDIE R. DAVIS AND WIFE, MARGARET DAVIS	9/3/1969	VOL. 1610, PAGE 356 PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION
41	ROBERT V. FAULK AND WIFE, SUSIE FAULK	9/2/1969	VOL. 1610, PAGE 345 PART OF LOT 6, BLOCK 8, HILLCREST SECOND ADDITION
42	C. J. ARENAS AND WIFE, EVA ARENAS	8/26/1969	VOL. 1610, PAGE 351 PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION
43	RUSSELL J. VAUGHAN AND WIFE, OLIVE VAUGHAN	9/3/1969	VOL. 1610, PAGE 353 PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION
44	VIRGINIA BURTON MCINNIS	8/25/1969	VOL. 1610, PAGE 343 PART OF LOT 7, BLOCK 8, HILLCREST SECOND ADDITION
45	ROBERT O. BEARDEN AND WIFE, RUBY M. BEARDEN	9/8/1969	VOL. 1611, PAGE 416 PART OF LOTS 6 & 7, BLOCK 8, HILLCREST SECOND ADDITION
46	OLIVER C. LAWSON AND WIFE, EDITH L. LAWSON	8/25/1969	VOL. 1612, PAGE 281 PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION
47	EDNA B. KING	10/1/1969	VOL. 1614, PAGE 34 PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION
48	EDNA B. KING, etal	10/1/1969	VOL. 1614, PAGE 24 PART OF LOT 6, BLOCK 8, HILLCREST SECOND ADDITION
49	HENRY J. HOPKINS AND WIFE, JOANN HOPKINS	4/29/1970	VOL. 1636, PAGE 231 PART OF LOT 6, BLOCK 8, HILLCREST SECOND ADDITION
50	EUGENIE G. RASHETA AND WIFE, BEVERLY ANN RASHETA	4/22/1970	VOL. 1936, PAGE 233 PART OF LOTS 6 & 7, BLOCK 8, HILLCREST SECOND ADDITION
51	BILLY M. BAILEY AND WIFE, CYNTHIA BAILEY	5/1/1970	VOL. 1636, PAGE 234 PART OF LOT 12, BLOCK 8, HILLCREST SECOND ADDITION
52	ROBERT E. SMITH AND WIFE, ALBURTHA MAE SMITH	5/5/1970	VOL. 1637, PAGE 351 PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION
53	JOHN E. BROADWAY AND WIFE, BETTYE L. BROADWAY	5/6/1970	VOL. 1637, PAGE 363 PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION
54	MARJORIE ANN ARENDALE	5/6/1970	VOL. 1637, PAGE 353 PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION
55	EDWARD L. PULLIN AND WIFE, VIRGINIA PULLIN	5/12/1970	VOL. 1638, PAGE 174 PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION
56	ROBERT H. BRADBERRY AND WIFE, MARION ELIZABETH BRADBERRY	5/12/1970	VOL. 1638, PAGE 176 PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION
57	PORT ARTHUR SAVINGS AND LOAN ASSOCIATION	5/12/1970	VOL. 1638, PAGE 183 PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION
58	CALVIN VINCENT AND WIFE, GERALDINE VINCENT	5/19/1970	VOL. 1640, PAGE 69 PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION
59	CARL E. FINLEY AND WIFE, FERN J. FINLEY	2/17/1972	VOL. 1722, PAGE 192 PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION
60	ALTON WILLIAMS AND WIFE, BERTHA WILLIAMS	5/27/1970	VOL. 1640, PAGE 197 PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION
61	JUNIUS P. BARKLEY AND WIFE, ELLIE NEAL BARKLEY	6/4/1970	VOL. 1641, PAGE 230 PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION
62	JOHN K. SATTLER AND WIFE, GLORIA SATTLER	6/1/1970	VOL. 1641, PAGE 300 PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION
63	VERNEST A. MILLER AND WIFE, ELLA MILLER	6/3/1970	VOL. 1642, PAGE 168 PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION
64	C. R. TYREE AND WIFE, IRENE TYREE	6/15/1970	VOL. 1642, PAGE 170 PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION
65	ALEX DORE AND WIFE, MAXNE DORE	11/25/1970	VOL. 1662, PAGE 323 PART OF LOTS 6 & 7, BLOCK 8, HILLCREST SECOND ADDITION
66	THOMAS H. WELLS AND WIFE, MARNELL WELLS	11/9/1970	VOL. 1662, PAGE 329 PART OF LOT 12, BLOCK 8, HILLCREST SECOND ADDITION
67	MARY ELIZABETH HARLE, etal	1/27/1971	VOL. 1668, PAGE 70 PART OF LOT 4 & 5, BLOCK 14, RANGE "K" PALCO SUB.
68	WILLIAM DOORNBOSS, etal	2/23/1971	VOL. 1671, PAGE 191 0.835 ACRE TRACT PART OF LOTS 8 & 9, BLOCK 8, HILLCREST SECOND ADDITION
69	W. R. FRASIER AND WIFE, MARY AGNES FRASIER	2/7/1972	VOL. 1720, PAGE 118 PART OF LOT 7, BLOCK 8, HILLCREST SECOND ADDITION
70	LAWRENCE T. APPLIGATE AND WIFE, NETHA APPLIGATE	2/10/1972	VOL. 1721, PAGE 460 PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION
71	ARVINA OPRY	2/10/1972	VOL. 1721, PAGE 463 PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION
72	JOHN C. BROWN AND WIFE, CRYSTAL L. BROWN	2/16/1972	VOL. 1722, PAGE 178 PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION
73	THOMAS E. EDGERLY, etal	2/8/1972	VOL. 1723, PAGE 142 TRACT ONE, PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION
74	THOMAS E. EDGERLY, etal	2/8/1972	VOL. 1723, PAGE 142 TRACT TWO, PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION
75	VIRGINIA BURTON MCINNIS	3/22/1972	VOL. 1727, PAGE 157 PART OF LOT 12, BLOCK 8, HILLCREST SECOND ADDITION
76	ROBERT T. MUNDAY AND PERCY ALEXANDAR MARSHALL	9/23/1972	VOL. 1753, PAGE 146 24.440 ACRES, PART OF LOTS 1, 2 AND 3, BLOCK 18, RANGE "M" PALCO SUB.
77	WILLIAM DOORNBOSS, etal	10/8/1973	VOL. 1806, PAGE 432 12.088 ACRES, PART OF LOTS 8, 9, 10 AND 11, BLOCK 8, HILLCREST SECOND ADDITION
78	GORDON R. PATE AND J. H. ARMSTRONG	2/2/1976	VOL. 1919, PAGE 478 PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION
79	VIRGINIA BURTON MCINNIS, etal	10/29/1970	VOL. 1663, PAGE 60 0.943 ACRES PART OF LOT 5, BLOCK 8, HILLCREST SECOND ADDITION
80	UNABLE TO LOCATE RECORDED INFORMATION AS TO OWNERSHIP OF THIS PARCEL		
81	J. J. TRF TICO	6/7/1955	432031 0.012 ACRES PARTS OF LOT 20, BEAUXART GARDENS
82	UNABLE TO LOCATE RECORDED INFORMATION AS TO OWNERSHIP OF THIS PARCEL		
83	WILLIAM DOORNBOSS, etal	11/9/1948	414315 PART OF LOTS 19 & 20, BLOCK 7, HILLCREST SECOND ADDITION
84	WILLIAM DOORNBOSS, etal	11/9/1948	414315 PART OF LOT 9, BLOCK 8, HILLCREST SECOND ADDITION
85	WILLIAM DOORNBOSS, etal	11/9/1948	414315 PART OF LOT 8, BLOCK 8, HILLCREST SECOND ADDITION
86	N. J. WIEMAN	6/16/1955	413718 LOT 30, SHAMROCK ACRES
87	JOHN E. RAGGIO	8/27/1955	VOL. 992, PAGE 120 PART OF LOTS 6 & 7, BLOCK 8, HILLCREST SECOND ADDITION
88	JOHN E. RAGGIO	8/27/1955	VOL. 992, PAGE 113 PART OF LOT 7, BLOCK 8, HILLCREST SECOND ADDITION
89	CARL E. FINLEY	6/15/1955	418005 LOT 27, SHAMROCK ACRES
90	WAYMON C. STAFFORD AND WIFE, MARIE ABSHIRE STAFFORD	10/25/1957	VOL. 1087, PAGE 132 LOT 45, SHAMROCK ACRES
91	A. VERNON WELSH AND WIFE, KATHERINE WELSH	4/4/1956	VOL. 1052, PAGE 271 0.003 ACRES, PART OF LOT 24, SHAMROCK ACRES
92	N. R. WESTERTERP AND WIFE, DICKIE LEE WESTERTERP	6/15/1955	VOL. 1728, PAGE 79 1.4 ACRES OUT OF LOTS 30 & 31, SHAMROCK ACRES
93	THOMAS E. EDGERLY AND WIFE, TILLIE C. EDGERLY	7/2/1970	VOL. 1045, PAGE 375 PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION
94	RICHARD E. DOORNBOSS, etal	3/7/2001	CF#2001008588 PARCEL 5A, 138.568 ACRES
95	CECIL A. DOMINY	11/28/1955	VOL. 1004, PAGE 628 PART OF LOTS 6 & 7, BLOCK 8, HILLCREST SECOND ADDITION
96	MRS. J. C. MCNABB, SR.	6/7/1955	430725 LOT 21, BEAUXART GARDENS
97	SIDNEY R. PLAKE AND WIFE, ANNA LOU PLAKE	6/7/1955	430150 LOT 62, BEAUXART GARDENS
98	ALMA MELDER	6/3/1955	430151 LOT 57, BEAUXART GARDENS
99	BASCO-MCALISTER LUMBER COMPANY	6/7/1955	438149 LOT 61, BEAUXART GARDENS
100	JOHN T. WILEY AND WIFE, DOROTHY WILEY	6/3/1955	483243 LOT 53, BEAUXART GARDENS
101	GLENN C. STROUP AND WIFE, DOROTHY M. STROUP	6/3/1955	441517 0.066 ACRES OUT OF LOT 54, BEAUXART GARDENS
102	DOROTHY LUCILLE SEHON, AND HAROLD SEHON		463285 0.016 ACRES OUT OF LOT 51, BEAUXART GARDENS
103	ELVA TIPPS ESTATE	2/13/2001	CF#2001006798 TRACT 1, 33.343 ACRES
104	ELVA TIPPS ESTATE	2/13/2001	CF#2001006798 TRACT 2, 0.473 ACRES
105	JO ANN HOLLIER, etal	5/1/2001	CF#2001015440 PARCEL 4, 32.02 ACRES
106	E. S. STEWART AND WIFE, NANNY LEE STEWART	10/25/1956	VOL. 1093, PAGE 446 PART OF LOT 20, BEAUXART GARDENS
107	NOLAN J. EAST, LUCILLE EAST AND ANNA REINTRA	10/3/1956	VOL. 1093, PAGE 440 LOTS 58, 59 AND 60, BEAUXART GARDENS
108	DOYLE PERKINS AND WIFE, EVA PERKINS	2/28/1949	VOL. 1060, PAGE 32 PART OF LOT 22, BEAUXART HOMESTEAD ASSOCIATION
109	VALENTE M. MARTINEZ AND WIFE, AMELIA S. MARTINEZ	6/12/1956	VOL. 1027, PAGE 418 LOT 28, SHAMROCK ACRES
110	WILLIAM R. TOUPS AND WIFE, PATSY RUTH TOUPS	10/26/1955	VOL. 998, PAGE 288 HOMESTEAD LOT 52, BEAUXART GARDENS
111	W. M. MCMULLEN AND WIFE, RUBY MCMULLEN	9/20/1955	VOL. 994, PAGE 23 LOT 28, SHAMROCK ACRES
112	JAMES R. BROWN AND WIFE, MARY E. BROWN	4/30/1956	VOL. 1025, PAGE 222 0.673 ACRES OUT OF LOT 25, SHAMROCK ACRES
113	THE LAGRONE TRUST	4/24/1956	VOL. 1023, PAGE 390 LOT 29, SHAMROCK ACRES
114	J. A. BROUSSARD AND WIFE, DOLORES BROUSSARD	1/9/1958	VOL. 1094, PAGE 366 LOTS 48 & 49, SHAMROCK ACRES
115	THOMAS E. EDGERLY, etal	7/2/1970	VOL. 1645, PAGE 379 PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION

EASEMENTS SHOWN ON DRAWING	
EASEMENT	RECORDING INFORMATION
LNVA FRESH WATER CANAL	VOL. 265, PAGE 359
CENTANA INTRASTATE PIPELINE COMPANY EASEMENT	CF#95-9638521
UNITED GAS TRANSMISSION COMPANY EASEMENT	VOL. 2202, PAGE 298
BHP PETROLEUM EASEMENT	CF#101-57-1254
GULF PIPELINE COMPANY EASEMENT	VOL. 340, PAGE 95
GULF STATES UTILITIES COMPANY EASEMENT	VOL. 806, PAGE 96
TEJAS POWER CORPORATION EASEMENT	CF#101-57-1254
NECHES CANAL COMPANY EASEMENT	VOL. 269, PAGE 423
TE PRODUCTIS PIPELINE COMPANY EASEMENT	CF#199903895
UNITED TEXAS TRANSMISSION COMPANY EASEMENT	CF#102-45-0918
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	VOL. 783, PAGE 381
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	VOL. 767, PAGE 98
CHICO OF TEXAS CORPORATION EASEMENT	VOL. 1474, PAGE 620
CHICO OF TEXAS CORPORATION EASEMENT	VOL. 1475, PAGE 588
SERVICE PIPELINE COMPANY EASEMENT	VOL. 1295, PAGE 565
GULF STATES UTILITIES COMPANY EASEMENT	VOL. 1619, PAGE 407
CITY OF PORT ARTHUR EASEMENT	VOL. 2248, PAGE 158
UNITED GAS PIPELINE EASEMENT	VOL. 837, PAGE 637
PORT GAS PROCESSING COMPANY EASEMENT	VOL. 1304, PAGE 164
TEXAS GAS CORPORATION EASEMENT	VOL. 1218, PAGE 323
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	VOL. 1433, PAGE 31
CITY OF PORT ARTHUR UTILITY EASEMENT	VOL. 1409, PAGE 311
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	VOL. 11, PAGE 153
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	VOL. 1881, PAGE 383
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	VOL. 1838, PAGE 268
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	VOL. 668, PAGE 231
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	CF#2000023255
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	CF#2001037549
JEFFERSON COUNTY DRAINAGE DISTRICT No. 7	CF#2001031848

BLANKET EASEMENTS AND EASEMENTS THAT ARE UNABLE TO BE PLOTTED AT THIS TIME AND ARE NOT SHOWN ON THIS DRAWING	
EASEMENT	RECORDING INFORMATION
COLONIAL PIPELINE COMPANY EASEMENT	VOL. 2152, PAGE 293
TEXAS AND NEW ORLEANS RAILROAD COMPANY EASEMENT	VOL. U, PAGE 552
UNITED GAS PIPELINE COMPANY EASEMENT	VOL. 840, PAGE 157
SERVICE PIPELINE COMPANY EASEMENT	VOL. 1301, PAGE 495
COLONIAL PIPELINE COMPANY EASEMENT	VOL. 1324, PAGE 501
DIXIE PIPELINE COMPANY EASEMENT	VOL. 297, PAGE 181
NATURAL GAS PIPELINE COMPANY EASEMENT	VOL. 1458, PAGE 181
DEFENSE PLANT CORPORATION EASEMENT	VOL. 538, PAGE 815
TEXAS EASTERN TRANSMISSION CORPORATION EASEMENT	VOL. 2274, PAGE 389
THE TEXAS PIPELINE COMPANY EASEMENT	VOL. 361, PAGE 207
THE TEXAS-EMPIRE PIPELINE COMPANY EASEMENT	VOL. 430, PAGE 146
SHELL PIPELINE CORPORATION EASEMENT	VOL. 517, PAGE 106
SERVICE PIPELINE COMPANY EASEMENT	VOL. 1151, PAGE 149
CENTANA INTRASTATE PIPELINE COMPANY EASEMENT	CF#96-9631372
CENTANA INTRASTATE PIPELINE COMPANY EASEMENT	CF#96-9631372
SHELL PIPELINE CORPORATION EASEMENT	VOL. 516, PAGE 118
DEFENSE PLANT CORPORATION EASEMENT	VOL. 532, PAGE 229
TEXAS EASTERN TRANSMISSION CORPORATION EASEMENT	VOL. 2007, PAGE 190



3010 Gaylord Parkway
 Suite 190
 Frisco, TX 75034
 (972) 377-7480

REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION



> 1 ASSOC CITY: BEAUMONT/PORT ARTHUR 4 STATE: TX LOC ID: BPT FAA SITE NR: 23416.*A
 > 2 AIRPORT NAME: JACK BROOKS RGNL 5 COUNTY: JEFFERSON TX
 3 CBD TO AIRPORT (NM): 09 SE 6 REGION/ADO: ASW/TEX 7 SECT AERO CHT: HOUSTON

GENERAL

10 OWNERSHIP: PUBLIC
 > 11 OWNER: JEFFERSON COUNTY
 > 12 ADDRESS: 1149 PEARL ST 4TH FLOOR
 BEAUMONT, TX 77701
 > 13 PHONE NR: 409-835-8466
 > 14 MANAGER: ALEX RUPP
 > 15 ADDRESS: 5000 JERRY WARE DR SUITE 100
 BEAUMONT, TX 77705
 > 16 PHONE NR: 409-719-4900
 > 17 ATTENDANCE SCHEDULE:

ALL ALL ALL

18 AIRPORT USE: PUBLIC
 19 ARPT LAT: 29-57-03.0000N ESTIMATED
 20 ARPT LONG: 094-01-14.5000W
 21 ARPT ELEV: 15.4 SURVEYED
 22 ACREAGE: 1,799
 > 23 RIGHT TRAFFIC: NO
 > 24 NON-COMM LANDING: NO
 25 NPIAS/FED AGREEMENTS: NGPY3
 > 26 FAR 139 INDEX: I A S 05/1973

RUNWAY DATA

> 30 RUNWAY INDENT:
 > 31 LENGTH:
 > 32 WIDTH:
 > 33 SURF TYPE-COND:
 > 34 SURF TREATMENT:
 35 GROSS WT: S
 36 (IN THSDS) D
 37 2D
 38 2D/2D2
 > 39 PCN:

LIGHTING/APCH AIDS

> 40 EDGE INTENSITY:
 > 42 RWY MARK TYPE-COND:
 > 43 VGSI:
 44 THR CROSSING HGT
 45 VISUAL GLIDE ANGLE:
 > 46 CNTRLN-TDZ:
 > 47 RVR-RVV:
 > 48 REIL:
 > 49 APCH LIGHTS:

OBSTRUCTION DATA

50 FAR 77 CATEGORY
 > 51 DISPLACED THR:
 > 52 CTLG OBSTN:
 > 53 OBSTN MARKED/LGTD:
 > 54 HGT ABOVE RWY END:
 > 55 DIST FROM RWY END:
 > 56 CNTRLN OFFSET:
 57 OBSTN CLNC SLOPE:
 58 CLOSE-IN OBSTN:

DECLARED DISTANCES

> 60 TAKE OFF RUN AVBL (TORA):
 > 61 TAKE OFF DIST AVBL (TODA):
 > 62 ACLT STOP DIST AVBL (ASDA):
 > 63 LNDG DIST AVBL (LDA):

SERVICES

> 70 FUEL: 100LL A
 > 71 AIRFRAME RPRS: MINOR
 > 72 PWR PLANT RPRS: MINOR
 > 73 BOTTLE OXYGEN: NONE
 > 74 BULK OXYGEN: LOW
 75 TSNT STORAGE: HGR, TIE
 76 OTHER SERVICES:
 AFRT, CHTR, INSTR, RNTL

FACILITIES

> 80 ARPT BCN: CG
 > 81 ARPT LGT SKED: SEE RMK
 BCN LGT SKED: SS-SR
 > 82 UNICOM: 122.950
 > 83 WIND INDICATOR: YES-L
 84 SEGMENTED CIRCLE: YES
 85 CONTROL TWR: YES
 86 FSS: MONTGOMERY
 87 FSS ON ARPT: NO
 88 FSS PHONE NR:
 89 TOLL FREE NR: 1-800-WX-BRIEF

BASED AIRCRAFT

90 SINGLE ENG: 60
 91 MULTI ENG: 8
 92 JET: 16
 TOTAL: 84
 93 HELICOPTERS: 5
 94 GLIDERS: 0
 95 MILITARY: 0
 96 ULTRA-LIGHT: 0

OPERATIONS

100 AIR CARRIER: 21
 102 AIR TAXI: 1,929
 103 G A LOCAL: 3,841
 104 G A ITNRNT: 9,413
 105 MILITARY: 1,071
 TOTAL: 16,275
 OPERATIONS FOR
 12 MONTHS
 ENDING: 12/31/2018

	12/30	16/34		
	6,750	5,070		
	150	150		
	CONC-G	ASPH-CONC-F		
	GRVD	GRVD		
	90.0	70.0		
	170.0	90.0		
	230.0	145.0		
	37 /R/D/X/U	26 /R/D/X/U		
	HIGH	HIGH		
	PIR - G / PIR - G	NPI - G / NPI - G	- / -	- / -
	/ V4L	V4L / V4L	/	/
	/ 54	52 / 53	/	/
	/ 3.00	3.00 / 3.00	/	/
	- / -	- / -	- / -	- / -
	T - N / R -	- / -	- / -	- / -
	/ Y	Y / Y	/	/
	MALSR /	/	/	/
	PIR / C	C / C	/	/
	/	/	/	/
	TREE / TREE	TREE /	/	/
	/	/	/	/
	46 / 41	48 /	/	/
	1,041 / 1,024	761 /	/	/
	618R / 529R	327L /	/	/
	18:1 / 20:1	11:1 / 50:1	/	/
	N / N	N / N	/	/
	6,750 / 6,750	5,070 / 5,070	/	/
	6,750 / 6,750	5,070 / 5,070	/	/
	6,675 / 6,750	5,070 / 5,070	/	/
	6,675 / 6,750	5,070 / 5,070	/	/

(>) ARPT MGR PLEASE ADVISE FSS IN ITEM 86 WHEN CHANGES OCCUR TO ITEMS PRECEDED BY >

> 110 REMARKS

A 033 RWY 16/34 FIRST 1000 FT RY 16 MUCH DARKER THAN REMAINDER OF RY.
 A 034 RY 16/34 FIRST 4271 FT OF RY 34 GRVD.
 A 039 RWY 16/34 FIRST 1,000 FEET OF RUNWAY 16 PCN REPORTED AS 28/F/D/X/U
 A 081 DUSK-DAWN. WHEN ATCT CLSD HIRL RY 12/30 & REIL RY 30; HIRL RY 16/34 & REIL RYS 16 & 34 PRESET MED INTST. ACTVT MALSR RY 12 - CTAF.
 A 110 THIS AIRPORT HAS BEEN SURVEYED BY THE NATIONAL GEODETIC SURVEY.
 A 110-001 BIRD ACTIVITY ON & INVOF ARPT.
 A 110-002 TWY HOLD SHORT SIGN FOR RWY 16/34 AND RWY 12/30 NOT LGTD. TWY C TWY LGT OTS. TWY C CLSD LGTD AND BARRICADED.
 A 110-003 FOR CD WHEN ATCT IS CLSD CTC HOUSTON APCH AT 281-443-5844 TO CNL IFR AT 281-443-5888.
 A 110-005 TWY G CLSD PERM.
 A 110-006 TWY H CLSD PERM.

111 INSPECTOR: (F) 112 LAST INSP: 01/17/2019 113 LAST INFO REQ:

Source: ACAIS
FAA Airports

Commercial Service Airports (Rank Order)
based on Calendar Year 2017 Enplanements

11/7/2018

326	EA	MD	HGR	Hagerstown	Hagerstown Regional-Richard A Henson Field	P	N	25,584	27,274	-6.20%
327	EA	WV	CKB	Clarksburg	North Central West Virginia	P	N	25,105	26,025	-3.54%
328	GL	MI	CMX	Hancock	Houghton County Memorial Santa Maria Public/Capt G Allan Hancock Field	P	N	24,843	25,439	-2.34%
329	WP	CA	SMX	Santa Maria	Pellston Regional Airport of Emmet County	P	N	24,527	40,120	-38.87%
330	GL	MI	PLN	Pellston	Key Field	P	N	23,961	25,497	-6.02%
331	SO	MS	MEI	Meridian	Jack Brooks Regional	P	N	23,923	26,359	-9.24%
332	SW	TX	BPT	Beaumont	Rhinelanders	P	N	23,767	26,518	-10.37%
334	GL	WI	RHI	Rhinelanders	Rhinelanders-Oneida County	P	N	23,014	20,414	12.74%
335	EA	NY	OGS	Ogdensburg	Ogdensburg International	P	N	22,957	10,281	123.30%
336	AL	AK	PSG	Petersburg	Petersburg James A Johnson	P	N	22,915	21,896	4.65%
337	GL	WI	EAU	Eau Claire	Chippewa Valley Regional	P	N	22,822	21,304	7.13%
338	EA	NY	ART	Watertown	Watertown International	P	N	22,785	17,312	31.61%
339	GL	MI	CIU	Sault Ste. Marie	Chippewa County International	P	N	22,261	20,974	6.14%
341	GL	MN	BRD	Brainerd	Brainerd Lakes Regional	P	N	21,383	18,328	16.67%
342	WP	CA	MMH	Mammoth Lakes	Mammoth Yosemite	P	N	21,352	21,826	-2.17%
343	WP	AZ	PGA	Page	Page Municipal	P	N	21,347	17,435	22.44%
345	WP	MP	GRO	Northern Islands (Municipality)	Benjamin Taisacan Manglona International	P	N	21,214	19,178	10.62%
346	GL	MN	STC	St. Cloud	St. Cloud Regional	P	N	20,918	15,615	33.96%
347	SO	KY	PAH	Paducah	Barkley Regional	P	N	20,910	20,266	3.18%
348	AL	AK	LHD	Anchorage	Lake Hood	P	N	20,689	23,382	-11.52%
349	NM	UT	OGD	Ogden	Ogden-Hinckley	P	N	20,324	15,609	30.21%
350	SO	KY	OWB	Owensboro	Owensboro-Daviess County Regional	P	N	19,578	23,537	-16.82%
352	SW	TX	GGG	Longview	East Texas Regional	P	N	19,261	22,480	-14.32%
353	GL	ND	DIK	Dickinson	Dickinson - Theodore Roosevelt Regional	P	N	18,963	16,822	12.73%
354	SO	PR	SIG	San Juan	Fernando Luis Ribas Dominicci	P	N	18,361	24,054	-23.67%
355	EA	PA	IPT	Williamsport	Williamsport Regional	P	N	18,323	19,320	-5.16%
356	AL	AK	MRI	Anchorage	Merrill Field	P	N	18,298	19,144	-4.42%
357	AL	AK	CDV	Cordova	Merle K (Mudhole) Smith	P	N	18,280	18,649	-1.98%
359	NM	WY	RKS	Rock Springs	Rock Springs-Sweetwater County	P	N	18,109	16,110	12.41%
360	GL	MI	IMT	Iron Mountain	Ford	P	N	17,707	12,604	40.49%
361	NM	WA	BFI	Seattle	Boeing Field/King County International	P	N	17,294	17,795	-2.82%
362	WP	NV	EKO	Elko	Elko Regional	P	N	17,115	14,893	14.92%
363	NE	RI	WST	Westerly	Westerly State	P	N	17,021	17,218	-1.14%
364	GL	MI	MKG	Muskegon	Muskegon County	P	N	17,007	18,165	-6.37%
365	GL	MI	ESC	Escanaba	Delta County	P	N	16,781	16,822	-0.24%
366	SW	NM	HOB	Hobbs	Lea County Regional	P	N	16,216	14,707	10.26%

Jack Brooks Regional Airport

Wildlife Hazard Management Plan

To comply with CFR 14 PART 139.337
As administered by the
Federal Aviation Administration

Alex Rupp
Airport Director

FAA APPROVED

APR 15 2019

MF

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Wildlife Hazard Management Plan 139.337

Jack Brooks Regional Airport (BPT), previously known as Southeast Texas Regional Airport and Jefferson County Regional Airport, (ICAO identifier KBPT), is a public use facility owned by Jefferson County. The airport is categorized as a FAR Part 139 facility.

The airport is located immediately east of Nederland, 3 miles northeast of Port Arthur, and 9 miles southeast of Beaumont, Texas in unincorporated Jefferson County.

A predominate amount of the daily operations are General Aviation, however the Airport is also served by American Airlines flying CRJ 200s.

Currently, land immediately adjacent to the airport on the east and southeast is the residential and business community of Nederland and Port Arthur. To the north is mixed non-specific use and the residential community of Beauxart Gardens. To the south is primarily agricultural, non-specific use with mixed business activities. To the west is primarily agricultural, wetland, and non-specific uses.

Entrance to the airport is via the airport access from Memorial Highway (TX 287/96/69) that runs immediately east of the airfield. Other significant roads surrounding the airport include West Port Arthur Avenue (TX 93) west of the airport and FM365, south of the airport.

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BPT WHMP Page 1 of 11

MF

I. Purpose

The specific goals of this document are:

- 1) Assign responsibility to organizations and individuals for carrying out specific actions at projected times and places in responding to a wildlife strike.
- 2) Identify personnel, equipment, facilities, supplies, and other resources available, within the airport for wildlife assessment and management.
- 3) Facilitate response and short-term recovery to set the stage for successful long-term recovery.

II. Implementation

The following personnel are in charge of assisting and carrying out the instructions that are given to aid in the reduction of wildlife hazards when necessary: **Maintenance / ARFF / Fuel Service Personnel.**

All maintenance personnel and ARFF Captains in charge of making wildlife observations throughout the day and report any issues immediately to the Operations Manager or ARFF Chief.

- Observation reportable issues during normal hours are:
 - Roosting wildlife
 - Nesting areas in hangars
 - Concentration of birds during mowing operation
 - Birds roosting on perimeter fences
 - Ground nesting in gravel areas or joints of concrete
 - Standing or ponding water
 - Assist in checking wildlife snares set by the state of Texas

III. State Law Enforcement

The State Wildlife Office aids the airport in wildlife hazard management. The State Wildlife Office provides, sets, and polices wildlife snare along the perimeter fence of the airport. The agent has access to the perimeter road through Gate #1 and #3.

IV. Federal Assistance

The Jack Brooks Regional Airport does not receive any Federal assistance in wildlife control. The airport intends to maintain a Migratory Bird Depredation Permit from the U.S. Fish and Wildlife Service. This permit allows controlling certain migratory birds specified in the permit on a year round basis.

V. Wildlife Attractants at Jack Brooks Regional Airport (BPT)

Per FAA Part 139 requirements, the BPT staff is responsible for ensuring that airfield habitat, surface water drainage, and perimeter fencing are managed to minimize bird and wildlife attractants.

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Preliminary observations of the AOA showed moderate interest and activity to minimize bird and mammal attractants. In addition, an excellent cooperative relationship exists between the airport, other airport users, and the airport staff is pursuing wildlife management techniques in accordance with the FAA Part 139 requirements.

The location of desirable habitat relative to the airport operations area (AOA) is also an important component in determining wildlife strike potential. When attractants are located on both sides of a runway or taxiway, wildlife will routinely travel between these areas during a daily period resulting in an increased strike hazard.

The airport and adjacent areas historically contained primarily tall/midgrass prairies along with wetlands, and flat wood forests, which are poorly drained flat sites that could have various different species mixtures. With the exception of numerous wildlife refuges and areas in the near vicinity of the airport, very little of this type of habitat still exists.

The current habitat consists largely of converted land communities with non-native grass species, annual forbs and woody plants, agricultural conversion, and oil business-related activities.

The current available wildlife habitat at the airport can be divided into four main groups: woodland/shrub land, water features, developed and disturbed sites, and agricultural land. Each one of these habitats at the airport has particular attractants that are responsible for sustaining potentially hazardous wildlife activity.

Vegetation is one attractant that all of these habitats have in common. Vegetation provides much of the food and cover requirements for wildlife. Mixed species of weeds, grasses, legumes, and fruit bearing woody plants provide food sources as well as nesting and roosting cover.

Woodland/shrub land habitat consists of both native and non-native species. The dominant species of trees on both the airfield and surrounding area include: Chinese Tallow (*Triadica sebifera*), oaks (*Quercus* spp.), Yaupon Holly (*Ilex vomitoria*), and willows (*Salix* spp.). The woodland/shrubland areas have a dense ground cover of Dewberry (*Rubus trivialis*) and Greenbriar (*Smilax* spp.). Wet marsh areas are commonly mixed throughout this cover type.

The south and southeast AOA and narrow stands along perimeter edges of the airport are lined by these woodland/shrub land habitats. These wooded areas adjacent to the airfield provide ideal habitat for many bird and mammal species, to include Coyotes, Bobcats, Feral Hogs, and raptors and contribute significantly to the potential wildlife risk on this airport.

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Revision Date: _____

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VI. Habitat Modifications and Changes

TASK	PROJECTED COMPLETION DATE:	ACTUAL COMPLETION DATE:
The Jack Brooks Regional Airport will make every attempt to change or modify the habitat to aid in the control of the wildlife on airport property. The airport is limited by the lack of jurisdiction over the privately owned property that surrounds the airport. The airport will attempt to inform the neighboring property owners whose property lies on runway approaches and encourage them not to enhance the habitat in these areas.	CONTINUAL	CONTINUAL
In the distance of two to five miles away from the airport various parcels of agriculture land and marshland exist. The airport will attempt to educate the owners of the agriculture land and encourage them to perform certain agriculture operations at night if at all possible. The marshland has been in existence for many years, the airport does not have the jurisdiction or the ability to modify.	CONTINUAL	CONTINUAL
The airport will, in accordance with FAA recommendations, keep the airfield turf maintained between 6-12 inches over the entire area to limit bird numbers and reduce maintenance costs. Grass will not be allowed to exceed 12 inches, when possible, and to go to seed; as such conditions will attract rodents and raptors/vulture predators. Selective application of herbicides may initially be necessary to eliminate weeds and allow grass to become reestablished in areas where it has been disturbed.	CONTINUAL	CONTINUAL
Once established, grass maintained at 6-12 inches will out-compete and thus reduce broad-leaved weed species. This will enable a reduction in the amount of broad-leaved herbicide applied to the field, if applicable. The last mowing of the growing season should be to top off the grass at 6 inches where it will dry and stand through the winter.	CONTINUAL	CONTINUAL
The airport removed three large brush piles located on the airport by having a contractor chip the piles into mulch.	09/30/2017	01/30/2019
The airport currently has two storm damaged hangars. Hangar 3 is scheduled to be demolished by 04/30/19 and Hangar 4 is scheduled to be demolished and replaced by a third-party by 06/30/19. With these hangars removed, we anticipate the reduction of possible nesting areas for birds and rodents.	06/30/2019	
Located on the west side of the airport are several acres of trees and brush that are scheduled to be removed which will reduce possible nesting areas for birds and rodents. The airport has established a timetable for the project, see Appendix 3. Update (Feb 2019): The Airport has been successful in obtaining a contractor with specialized tree removal equipment and over the last two years, we have removed approximately 36.5 acres of trees and brush. The remaining 73 acres of trees are scheduled to be removed over the next 3 years.	12/30/2022	

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VII. Immediate Action

The airport will do the following to aide in reduction of wildlife.

- Maintain grass in safety areas grass height no less than 5 inches
 - Maintain grass no less than 2 inches around Taxiways and Runway edge lights and sign boxes.
 - Maintain ramps and AOA areas free of small gravel
 - Maintain ramps and AOA areas joint seals clean of grass
 - Provide efforts to eliminate nesting areas for birds
 - Tractor operators will be supplied with pyrotechnics to disperse wildlife during mowing operations

VIII. Long term habitat modification

The short term goal of the airport is to remove all trees within the perimeter fence and the Runway 30 Approach property, see Appendix 3 for the Tree Removal Plan.

The long term goal of the airport is to maintain the areas of tree removal to restrict future tree growth.

IX. Wildlife permits

The Jack Brooks Regional Airport will obtain U.S. Fish and Wildlife Services - Migratory Bird Depredation Permits. The permit allows controlling certain migratory birds specified in the permit on a year round basis. The permitted birds are: Morning Doves, Cattle Egret, Killdeer, and Laughing Gull.

Active permits can be found in Appendix 4.

X. Pesticides

The Jack Brooks Regional Airport does not use pesticides to control wildlife.

XI. Airport Personnel / Equipment

The Jack Brooks Regional Airport will provide airport personnel necessary to control wildlife, maintain the airport, and to prevent or possibly reduce aircraft strikes. The airport has 8 full time maintenance personnel that are on duty between the hours of 6 A.M. to 4 P.M., Monday through Friday. The airport has 9 ARFF personnel that are on duty through various shifts 24 hours a day, 365 days a year.

Available Equipment / Supplies:

- 2 - Kubota tractors equipped with airfield radios
- 2 - 15ft Rhino mower decks.
- 1 - 6ft Rhino mower
- 4 - ZTR mowers
- 1 - 1000.00 Gal spray rig with 50FT booms and gps marking system

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- 1 – Tymco Sweeper with airfield radio
- 8 – Pickup trucks with airfield radios
- 2 – ARFF trucks with airfield radios
- 1 – Club car ATV
- 1 - 12GA shotgun
- 4 - pyrotechnic launcher
- Several boxes of pyrotechnic shells (screamers, bangers)
- Primers for pyrotechnic launcher
- Several boxes of steel shot 12GA shotgun shells

All supplies are kept under lock and key, only accessible by the Operation Chief, ARFF Chief or Airport Director.

XII. Assignment of Personnel for Implementing the Plan

It is the duty of all airport personnel to report wildlife activity to the Operation Chief and at that point he will advise what needs to be done. If the Operation Chief is not available, then personnel are to contact either the ARFF Chief or Airport Director. If personnel are unable to contact any member of the management team, they have the authority to activate the plan to maintain a safe environment for the flying public.

XIII. Inspection for Wildlife

Throughout the day beginning at dawn airport personnel will make wildlife observations during daylight operations. During these observations personnel should focus on all ramps, AOA areas, and safety areas. At any time a significant amount of wildlife has been spotted it is to be reported. At any time ATC reports birds, personnel will immediately inspect the area to determine what action is needed, if any to correct the issue.

XIV. Wildlife Control Measures

When a wildlife hazard has been reported, an inspection will take place and the proper means of control will be made. The procedures listed below are to be followed to determine course of action.

- Identify the type of wildlife.
- Is it an endanger species?
- Do we have a permit for the species if lethal action is needed?

What is the easiest way to disperse the wildlife without doing harmful impact to the environment?

Once the procedure listed above has been vetted, then personnel shall proceed with whatever course of action listed in the plan to make the area safe.

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XV. Approved Action for Controlling Wildlife

Once the hazard has been identified, detraction can be made by the following actions:

- Contact ATC to see if ANY aircraft are about to land or take off.
- If you are on the ramps consider where is aircraft parked?
- First line of action is harassment- use horns, sirens, and water
- Second line of action- consults with ATC again on aircraft traffic; if clear then proceed with pyrotechnics. (screamers and bangers)
- Third line of action is lethal action- consults with ATC again on aircraft traffic, identify what is behind your target, and determine if it is safe to discharge a firearm?
 - Point the weapon in a safe direction and load the weapon with NON TOXIC STEEL SHOT.
 - Double check the area to ensure the area is safe to precede, if safe discharge the firearm.
 - Once lethal action has been taken any type of wildlife is to be removed and disposed of in a proper manor. The employee shall wear GLOVES to prevent any type of exposure to diseases that the animal may have.
 - Once the hazard no longer exists, relay the information to the ATC and airport Management.

XVI. Training

The JBRA has a training program that is a regular part of our 139 training. This training is done a minimum of every 12 consecutive calendar months. The training is also reviewed if a triggering event occurs.

XVII. Evaluation and Review of the WHMP

The airport will review the WHMP a minimum of once every 12 calendar months and will be a part of the regular plan review cycle at the airport. The airport will hold an annual meeting to discuss any other hazards that may have arisen to become an issue. If an aircraft incident occurs it will automatically trigger a review of the plan.

When the annual review is being performed the following form will be used to aid in the review.

FAA APPROVED

Original Date: _____

FAA Approval: _____ APR 15 2019

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Appendix 1. WHMP Annual Review

Date: _____

Subject: Wildlife Hazard Management Plan Annual Review

Airport: JACK BROOKS REGIONAL AIRPORT

Airport ID: BPT

On _____ we conducted the annual review the Wildlife Hazard Management Plan, as per the requirements of 139.337 (f) (6).

General Information/Significant findings:

Name of review coordinator : _____ (Person facilitating discussions and writing plan updates; usually the Wildlife Coordinator, Wildlife Biologist, or Airport Director) & participating airport personnel and representatives of other organizations (As listed in 139.337 (f) (1); may include members of airport management, the wildlife coordinator, airport operations/wildlife staff, wildlife Biologist who conducted Wildlife Hazard Assessment, members of the wildlife hazard working group*). **Attach a sign-in sheet.**

Summary of results of annual data analysis:

- -
- -
- -
- -
- -
- -

Example: ranking of highest priority species based on the analysis. (Per standardized continual monitoring procedures of 139.337 (f) (6); data for analysis may include logs of wildlife strikes, wildlife observations and control measures, standardized wildlife monitoring surveys, and wildlife data from off – airport sites of concern.)

Summary of progress and challenges in management of the most significant wildlife attractants and / or habitats on or near the airport – (Review of habitat management proprieties listed in 139.337 (f) (2)).

Summary of progress and challenges in direct wildlife hazard management (i.e., dispersals, strike response) on the airfield – (Review of procedures to be followed during air carrier operations as listed in 139.337 (f)(5)).

Changes to management strategies identified and changes to documentation identified.

Changes to Wildlife Hazard Working Group membership or objectives identified and changes to airport training program identified.

Changes/ updates to Wildlife Hazard Management Plan Identified

(Submit any changes to the WHMP to the assigned FAA Airport Certification Safety Inspector)

Airport Operations Manager

FAA APPROVED

Original Date: _____

FAA Approval: _____ **APR 15 2019**

Revision Date: _____

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Appendix 2. WHMP Review Following a Triggering Event

Date: _____

Subject: Wildlife Hazard Management Plan Review Following a Triggering Event

Airport: JACK BROOKS REGIONAL AIRPORT

Airport ID: BPT

On _____ we conducted a review the Wildlife Hazard Management Plan, as per the requirements of 139.337 (f) (6).

Description of Triggering Event:

Date/Time – Provide details of the event which triggered the review. Attach strike report, if available and any pertinent information; runway used, airline, take-off, landing, species, damage, etc.

General Information/ Significant findings:

Name of review coordinator – (Person facilitating discussions and writing plan updates; usually the Wildlife Coordinator, Wildlife Biologist, or Airport Director) & participating airport personnel and representatives of other organizations (As listed in 139.337 (f) (1); may include members of airport management, the wildlife coordinator, airport operations/ wildlife staff, wildlife Biologist who conducted Wildlife Hazard Assessment, members of the wildlife hazard working group*). Attach a sign – in sheet.

The plan’s effectiveness in dealing with known wildlife hazards on and in the airport’s vicinity – Example: Review the current wildlife control log and evaluate recent strike reports or events. Make a determination as to whether the current program is working and what can be improved.

Aspects of the wildlife hazards described in the wildlife hazard assessment that should be reevaluated – Review assessment to determine if everything is being addressed that was previously identified as a hazard or if other species are now present. Note: if other/additional new species are now present on or in the vicinity of the airport, another Wildlife Hazard Assessment may be needed.

Summary of progress and challenges in direct wildlife hazard management (i.e. dispersals, strike response) on the airfield – (Review of procedures to be followed during air carrier operations as listed in 139.337 (f)(5))

Changes to: management strategies identified, airport training program identified

Changes/updates to Wildlife Hazard Management Plan identified

(Submit any changes to the WHMP to the assigned FAA Airport Certification Safety Inspector)

Airport Operations Manager

FAA APPROVED

Original Date: _____

FAA Approval: _____ **APR 15 2019**

Revision Date: _____

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Appendix 3. Tree Removal Plan



The tree areas shaded in orange above were removed Oct 2017 (11.2 acres)

The tree areas shaded in white above were removed Dec 2018 (22.5 acres)

The airport plans to remove the red areas (17 acres) Dec 2019, blue area (37.5 acres) Dec 2020, and then the green area (19.2 acres) Dec 2021.

FAA APPROVED

Original Date: _____

FAA Approval: _____ **APR 15 2019**

Revision Date: _____

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Appendix 4. US Fish and Wildlife Depredation Permit

(Attach Current Depredation Permit Behind This Page)

Original Date: _____

Revision Date: _____

FAA APPROVED
FAA Approval: _____
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