



JEFFERSON COUNTY PURCHASING DEPARTMENT
Deborah L. Clark, Purchasing Agent

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Addendum to IFB

IFB NUMBER: IFB 26-024/MR
IFB TITLE: Jack Brooks Regional Airport Airfield Lighting Rehabilitation
IFB DUE BY: 11:00 am CT, Wednesday, June 17, 2026
ADDENDUM NO.: 3
ISSUED (DATE): June 10, 2026

To Bidder: This Addendum is an integral part of the IFB package under consideration by you as a Bidder in connection with the subject matter herein identified. Jefferson County deems all sealed bids to have been proffered in recognition and consideration of the entire IFB Specifications Package – including all addenda. For purposes of clarification, receipt of this present Addendum by a Bidder should be evidenced by returning it (signed) as part of the Bidder’s sealed bid submission. If the bid submission has already been received by the Jefferson County Purchasing Department, Bidder should return this addendum in a separate sealed envelope, clearly marked with the IFB Title, IFB Number, and IFB Opening Date and Time, as stated above.

Reason for Issuance of this Addendum:

- 1. Plan & Specification revisions from Garver, USA
2. Revised Bid Form.

The information included herein is hereby incorporated into the documents of this present bid matter and supersedes any conflicting documents or portion thereof previously issued.

Receipt of this Addendum is hereby acknowledged by the undersigned Respondent:

ATTEST:

Witness

Witness

Approved by Date:

Authorized Signature (Respondent)

Title of Person Signing Above

Typed Name of Business or Individual

Address



21 Waterway Ave.
Suite 300 Office 348
The Woodlands, TX 77380

www.GarverUSA.com

ADDENDUM NO. 3

Date: June 10, 2026
Project Name: Airfield Lighting Rehabilitation Project
Owner: Jefferson County
Jack Brooks Regional Airport
Owner Project No: Jefferson County Project IFB 26-024/MR
Garver Project No: 2400880

This addendum shall be a part of the Plans, Contract Documents and Specifications to the same extent as though it were originally included therein, and it shall supersede anything contained in the Plans, Contract Documents, and Specifications with which it might conflict. This addendum, including all attachments, shall become part of the Contract and all provisions of the Contract shall apply thereto. The time provided for completion of the Contract has not been changed as noted in this addendum. Acknowledgement of receipt of this addendum must be noted in the appropriate section of the Bid Form and included with the Contract Documents.

A. SPECIFICATIONS

1. Remove and replace the Bid Form with the replacement Bid Form attached hereto.
2. Remove the following specifications sections in their entirety, and replace with the same, attached hereto:
 - a. SS-301 – Electrical Demolition and Relocation Work
 - b. L-115 – Electrical Manholes and Junction Structures
3. Revise Section 00 72 00 – General Provisions as follows:
 - a. Revise paragraph 70-15 by deleting “John Doe” and replacing with “Bruce Hyers”
 - b. Revise paragraph 70-15 by deleting “(555) 555-5555” and replacing with “(615) 717-8592”

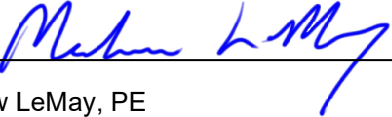
B. STANDARD DETAILS

1. No items for Standard Details are included in this addendum.

C. DRAWINGS

1. Remove the following drawings in their entirety, and replace with the same, attached hereto:
 - a. GC001 – Construction Safety & Phasing Notes 1
 - b. ED106 – Lighting Demolition Plan 6
 - c. EL507 – Lighting Installation Details 7

**Addendum No. 3 – Airfield Lighting Rehabilitation Project
June 10, 2026**

By: 

Matthew LeMay, PE
Senior Project Manager



Digitally Signed 06/10/2026

Attachments:
Specifications

Bid Form

SS-301 – Electrical Demolition and Relocation Work

L-115 – Electrical Manholes and Junction Structures

Drawings

GC001 – Construction Safety & Phasing Notes 1

ED106 – Lighting Demolition Plan 6

EL507 – Lighting Installation Details 7

END OF ADDENDUM NO. 3



**JACK BROOKS REGIONAL AIRPORT
BPT AIRFIELD LIGHTING REHABILITATION
UNIT PRICES - BASE BID**

ITEM NO.	SPEC. NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	UNIT PRICE	AMOUNT
1	SS-120-3.1	Construction Safety and Security	LS	1		
2	SS-300-5.1	Lockout/Tagout and Constant Current Regulator Calibration Procedures	LS	1		
3	SS-300-5.2	Electrical Vault Modifications	LS	1		
4	SS-300-5.3	Existing Lighting Regulator, Removed	EA	3		
5	SS-300-5.4	L-829, 10 kW, Lighting Regulator, Installed	EA	2		
6	SS-300-5.5	L-829, 7.5 kW, Lighting Regulator, Installed	EA	2		
7	SS-300-5.6	Generator and Transfer Switch System, Installed	LS	1		
8	SS-300-5.7	ALCMS Modifications	ALLOW	1	\$ 40,000.00	
9	SS-301-5.1	Existing Base Mounted Runway Edge Light, Removed	EA	97		
10	SS-301-5.2	Existing Flush In-Pavement Runway Edge Light, Removed, Blank Cover Installed	EA	9		
11	SS-301-5.3	Existing Base Mounted Guidance Sign, Removed	EA	33		
12	SS-301-5.4	Existing Base Mounted Guidance Sign, Removed, Base to Remain	EA	7		
13	SS-301-5.5	Existing Base Mounted Taxiway Edge Light, Removed, Base to Remain	EA	34		
14	SS-301-5.6	Existing Base Mounted Taxiway Edge Light, Removed	EA	56		
15	SS-301-5.7	Existing Runway End Identification Light System, Removed	EA	3		
16	SS-301-5.8	Existing Base Mounted Runway Threshold Lights, Removed	EA	24		
17	SS-301-5.9	Existing Base Mounted Runway Threshold Lights with Base to Remain, Removed	EA	8		
18	SS-301-5.10	Existing Generator and Transfer Switch, Removed	LS	1		
19	SS-301-5.11	Existing Airport Rotating Beacon Fixture, Removed	LS	1		
20	SS-301-5.12	Existing Base Mounted Runway Edge Light with Base to Remain, Removed	EA	10		
21	SS-301-5.13	Conduit or Duct Bank, Cleared – per Linear Foot	LF	10,350		
22	SS-301-5.14	Abandoned Sign Base, Removed	EA	1		
23	SS-301-5.15	Existing Handhole, Removed	EA	2		
24	SS-305-5.1	Directional Boring, 1-Way 2" Polyethylene Conduits	LF	1,400		
25	SS-305-5.2	Directional Boring, 2-Way 2" Polyethylene Conduits	LF	1,800		
26	SS-310-5.1	Flight Check Mobilization	EA	1		
27	SS-310-5.2	Temporary Airfield Lighting	LS	1		
28	C-105-6.1	Mobilization (Maximum 5% of Total Bid)	LS	1		
29	L-101-5.1	L-802A(L), Class I Airport Rotating Beacon, In Place	EA	1		
30	L-108-5.1	No. 8 AWG, 5 kV, L-824, Type C Cable, Installed in Trench, Duct Bank or Conduit	LF	51,500		

31	L-108-5.2	No. 6 AWG, Solid, Bare Copper Counterpoise Wire, Installed Above the Duct Bank or Conduit, Including Connections/Terminations	LF	31,400		
32	L-110-5.1	Non-Encased Electrical Conduit, 1W-2"	LF	25,100		
33	L-110-5.2	Concrete Encased Electrical Conduit, 1W-2" with Sawkerf Pavement Repair	LF	700		
34	L-110-5.3	Non-Encased Electrical Conduit, 2W-2"	LF	650		
35	L-115-5.1	Electrical Junction Structure, L-867 Class 1, Size 16" Diameter by 24" Depth	EA	11		
36	L-115-5.2	Electrical Junction Structure Plaza, Two L-867 Class 1, Size 16" Diameter by 24" Depth Cans	EA	10		
37	L-115-5.3	3'x4' Airfield Handhole Cover Replacement	EA	3		
38	L-125-5.1	L-849A(L) Runway End Identification Lights System, Installed on Runway 16	LS	1		
39	L-125-5.2	L-849A(L) Runway End Identification Lights System, Installed on Runway 34	LS	1		
40	L-125-5.3	L-849A(L) Runway End Identification Lights System, Installed on Runway 30	LS	1		
41	L-125-5.4	L-850C(L) Low Profile Base Mounted In-pavement Runway Edge Light, Installed In Existing Full Strength Pavement	EA	8		
42	L-125-5.5	L-862(L) Base Mounted Runway Edge Light, Installed	EA	98		
43	L-125-5.6	L-862(L) Base Mounted Runway Edge Light, Installed on Existing Base	EA	10		
44	L-125-5.7	L-861T(L) Base Mounted Taxiway Edge Light, Installed	EA	4		
45	L-125-5.8	L-861T(L) Base Mounted Taxiway Edge Light, Installed on Existing Base	EA	34		
46	L-125-5.9	L-862E(L) Base Mounted Runway Threshold Light, Installed	EA	24		
47	L-125-5.10	L-862E(L) Base Mounted Runway Threshold Light, Installed on Existing Base	EA	8		
48	L-125-5.11	L-858 Base Mounted, 1-Module Guidance Sign, Installed	EA	9		
49	L-125-5.12	L-858 Base Mounted, 1-Module Guidance Sign, Installed on Existing Base	EA	1		
50	L-125-5.13	L-858 Base Mounted, 2-Module Guidance Sign, Installed on Existing Base	EA	4		
51	L-125-5.14	L-858 Base Mounted, 3-Module Guidance Sign Installed on Existing Base	EA	2		
52	L-125-5.15	L-858 Base Mounted, 4-Module Guidance Sign,	EA	1		
53	P-101-5.2	Pavement Marking Removal	SF	2,080		

TOTAL BID - BASE BID _____

ITEM SS-301 ELECTRICAL DEMOLITION WORK

DESCRIPTION

301-1.1 This item shall consist of the removal and satisfactory disposal of existing runway and taxiway edge lights, in-pavement lights, guidance signs, markers, manholes, handholes, junction structures, racks, pads, equipment, poles, towers, shelters, and other incidentals, all of which are not designated or permitted to remain, in accordance with this specification, the referenced specifications and drawings, and applicable advisory circulars. This work shall include the removal of indicated equipment, materials, and incidentals necessary for a complete item removal, including all restoration work, as a completed unit to the satisfaction of the Engineer.

301-1.2 The Contractor shall maintain current copies of all referenced and applicable standards on the job site. The Contractor is responsible to make known to the Engineer any conflict between plans and specifications that he observes or of which he is made aware.

301-1.3 Additional details pertaining to the lighting system covered in this item are contained in the advisory circular, AC 150/5340-30, Design and Installation Details for Airport Visual Aids. Additional details pertaining to the PAPI system covered in this item are contained in the advisory circular, AC 150/5345-28, Standard for Precision Approach Path Indicator (PAPI) Systems. Additional details pertaining to the REIL system covered in this item are contained in the advisory circular, AC 150/5345-51, Specification for Discharge-Type Flasher Equipment.

MATERIALS

301-2.1 All backfill and repair materials used in electrical demolition, repair and restoration work shall comply with the referenced specifications and be approved by the Engineer.

Airport lighting equipment and materials shall meet the requirements outlined in Item SS-300 and Item L-125.

For pre-cast or prefabricated concrete encased light base installations, the Contractor shall submit and coordinate the construction of the proposed pre-cast units with the Engineer onsite to review and approve the construction process. The Contractor shall submit his proposed installation process for review and approval by the Engineer. The Contractor shall provide additional items and work if required and requested by the Engineer for the construction and installation of the pre-cast units at no additional cost to the Owner.

CONSTRUCTION METHODS

301-3.1 General. No demolition shall be started until the removal and/or relocation work has been laid out and approved by the Engineer. All material shall be disposed of off-site. All hauling and disposal will be considered a necessary and incidental part of the work. Hauling cost shall be considered by the Contractor and included in the contract unit price for the pay of items of work involved. No payment will be made separately or directly for hauling on any part of the work.

Equipment removal shall typically include removing the equipment and its accessories, removing foundations/pads, removing existing conduits, conductors and appurtenances, removal of conduit to below grade, and removal of existing circuits back to source. The work shall include restoring the area to match existing, including filling and tamping all holes with earth, and clearing and leveling the site.

The Contractor shall remove all existing underground cable, which is unused or rendered unusable by this project, when such is exposed or made accessible during this work. All such wiring removed shall become property of the Contractor and shall be immediately removed from the project. Wiring in conduit shall be

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BPT Airfield Lighting Rehabilitation

removed as indicated or if new wiring is shown to be installed in its place. Existing wiring shall not be reused or reinstalled.

Wiring not exposed shall be abandoned in place, if a reasonable effort will not remove it. No measurement or payment will be made for this cable removal work. Damage to turf or other systems will not be permitted to salvage or retrieve existing cable.

Any damage to electrical equipment, systems, structures, conduits, cables, and accessories or other utilities, designated to remain in place, shall be repaired or replaced expeditiously at no additional cost to the Owner and to the satisfaction of the Owner and Engineer.

Holes, ditches, or other abrupt changes in elevation caused by the removal operations that could obstruct drainage or be considered hazardous or unsightly shall be backfilled, compacted, and left in a workmanlike condition.

Trenches or voids resulting from the removal or demolition of existing electrical equipment or other structures shall be filled with approved material placed in layers in accordance with Item P-152.

Concrete foundations and pads to be removed shall be obliterated full depth.

301-3.2 Removal and/or relocation of light fixtures and equipment. Light fixtures and other equipment which are to be removed shall be carefully excavated. All concrete bases and concrete anchors shall be removed by the Contractor. The removed lights, guidance signs, isolation transformers and wiring harnesses shall then be given to the Owner, or properly disposed of if so directed by the Owner. The ground around the removed lighting equipment shall be backfilled and properly compacted. Light fixtures and equipment which are to be relocated shall be stored on site and reinstalled with new lamps, new transformers, and all other new required accessories as indicated in the plans.

301-3.3 Removal of existing equipment. The Contractor shall carefully remove all salvageable equipment as indicated in the plans. Any equipment that is damaged during the removal and/or relocation operation shall be subject to a reduction in payment for removal and/or relocation of the equipment. All equipment that is removed during this project shall be transported to a site on the Airfield or removed from the Airfield and properly disposed of as directed by the Owner and the Engineer.

301-3.4 Relocation of existing equipment. Existing equipment that is to be relocated shall be carefully disconnected from the existing electrical system. The equipment shall be stored on site in an enclosed area protected from the weather as directed by the Owner and Engineer. The Contractor shall remove existing concrete bases and shall backfill and compact these areas to match existing. The electrical power circuit shall be field located and extended to the new installation location unless otherwise noted in the Plans. Coordinate the extension of the electrical service with the extension of the electrical duct serving the equipment and install duct, splice and cable markers to mark the new complete route.

Refer to the plans for additional installation requirements concerning the relocation of existing lights, signs, systems and incidentals.

301-3.5 Clearing of existing duct banks or conduits. Where new cable is to be installed in an existing duct bank or conduit, the full length of the duct bank or conduit shall be cleared of debris by mechanical means before installation of the new cable. Acceptable means of clearing existing duct banks or conduits may include using a mandrel, "hydro-jetting" method, or "roto-rooting" method as approved by the Engineer.

Where existing cables are being removed from an existing duct bank or conduit and replaced with new cables, the Contractor shall use the existing cable to clear the conduit and to pull in the new cable. No separate payment will be made for clearing these duct banks or conduits or for the existing cable removal. Clearing of these existing duct banks or conduits is incidental to the cable pay item unless otherwise noted.

METHOD OF MEASUREMENT

301-4.1 The quantity of existing duct bank or conduit, cleared, to be measured under this item, shall be the linear foot of duct bank or conduit cleared using a mandrel or other approved method to remove debris, including swabbing and clearing of each conduit in a duct bank, and installation of pull wires in spare conduits. Separate measurement shall be made for the various types and sizes of duct bank or conduit. Partial payment will not be made for duct bank or conduit that cannot be cleared using a method acceptable to the Engineer. Separate payment will not be made for existing cable removal.

301-4.2 The quantity of existing lights or guidance signs, removed, to be measured under this item shall be the number of each complete unit removed, and accepted by the Engineer.

This item shall include removing and storing the existing equipment as directed by the Engineer.

Where the light base and concrete structure are indicated to be removed or demolished, the item shall include restoring the area to match existing, including removing the complete concrete item, filling and tamping all holes with earth (brought from off-site if necessary), and clearing and leveling the site.

Where the light base and concrete structure are to remain, a new blank cover shall be installed for protecting the light base during the construction work. Blank covers shall be removed when the existing equipment is reinstalled and given to the Owner after completion of construction work in the respective area.

301-4.3 Measurement for the removal of the REIL system shall be the number of each removed for a completed and accepted removal of a complete REIL system. Equipment shall be carefully removed and stored as directed by the Engineer and Owner. Concrete bases and appurtenances shall be demolished and removed. The area shall be restored to match existing including seeding and topsoiling. This item shall also include modifications to the current REIL power feeder, including new circuit breaker, labeling, waterproof cast splicing, splice marker and all required appurtenances to reconnect the power feeder to only serve the existing REIL equipment to the satisfaction of the Engineer.

301-4.4 Measurement for the removal of the Beacon Fixture shall be the number of each removed for a completed and accepted removal of a complete Beacon Fixture. Equipment shall be carefully removed and stored as directed by the Engineer and Owner.

BASIS OF PAYMENT

301-5.1 Payment will be made at the contract unit price for each complete item, measured as provided above, and accepted by the Engineer. This price shall be full compensation for furnishing all materials and for all preparation, assembly, and removal of these materials, and for all labor, equipment, tools, and incidentals necessary to complete this item to the satisfaction of the Engineer.

Payment will be made under:

- Item SS-301-5.1 Existing Base Mounted Runway Edge Light, Removed -- per Each
- Item SS-301-5.2 Existing Flush In-Pavement Runway Edge Light, Removed, Blank Cover Installed -
- per Each
- Item SS-301-5.3 Existing Base Mounted Guidance Sign, Removed -- per Each
- Item SS-301-5.4 Existing Base Mounted Guidance Sign, Removed, Base to Remain -- per Each
- Item SS-301-5.5 Existing Base Mounted Taxiway Edge Light, Removed, Base to Remain -- per
Each

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- Item SS-301-5.6 Existing Base Mounted Taxiway Edge Light, Removed -- per Each
- Item SS-301-5.7 Existing Runway End Identification Light System, Removed -- per Each
- Item SS-301-5.8 Existing Base Mounted Runway Threshold Lights, Removed -- per Each
- Item SS-301-5.9 Existing Base Mounted Runway Threshold Lights with Base to Remain, Removed – per Each
- Item SS-301-5.10 Existing Generator and Transfer Switch, Removed – per Lump Sum
- Item SS-301-5.11 Existing Airport Rotating Beacon Fixture, Removed - per Lump Sum
- Item SS-301-5.12 Existing Based Mounted Runway Edge Light with Base to Remain, Removed – per Each
- Item SS-301-5.13 Conduit or Duct Bank, Cleared – per Linear Foot
- Item SS-301-5.14 Abandoned Sign Base, Removed – per Each
- Item SS-301-5.15 Existing Handhole, Removed – per Each**

END OF ITEM SS-301

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ITEM L-115 ELECTRICAL MANHOLES AND JUNCTION STRUCTURES

DESCRIPTION

115-1.1 This item shall consist of electrical manholes and junction structures (hand holes, pull boxes, junction cans, etc.) installed per this specification, at the indicated locations and conforming to the lines, grades and dimensions shown on the plans or as required by the RPR. This item shall include the installation of each electrical manhole and/or junction structures with all associated excavation, backfilling, sheeting and bracing, concrete, reinforcing steel, ladders, appurtenances, testing, dewatering and restoration of surfaces to the satisfaction of the RPR.

Refer to Item SS-301 for additional electrical demolition work requirements.

EQUIPMENT AND MATERIALS

115-2.1 General.

a. All equipment and materials covered by referenced specifications shall be subject to acceptance through manufacturer's certification of compliance with the applicable specification when so requested by the RPR.

b. Manufacturer's certifications shall not relieve the Contractor of the responsibility to provide materials per these specifications. Materials supplied and/or installed that do not comply with these specifications shall be removed (when directed by the RPR) and replaced with materials that comply with these specifications at the Contractor's cost.

c. All materials and equipment used to construct this item shall be submitted to the RPR for approval prior to ordering the equipment. Submittals consisting of marked catalog sheets or shop drawings shall be provided. Submittal data shall be presented in a clear, precise and thorough manner. Original catalog sheets are preferred. Photocopies are acceptable provided they are as good a quality as the original. Clearly and boldly mark each copy to identify products or models applicable to this project. Indicate all optional equipment and delete any non-pertinent data. Submittals for components of electrical equipment and systems shall identify the equipment to which they apply on each submittal sheet. Markings shall be made bold and clear with arrows or circles (highlighting is not acceptable). The Contractor is solely responsible for delays in the project that may accrue directly or indirectly from late submissions or resubmissions of submittals.

d. The data submitted shall be sufficient, in the opinion of the RPR, to determine compliance with the plans and specifications. The Contractor's submittals shall be electronically submitted in pdf format, tabbed by specification section. The RPR reserves the right to reject any and all equipment, materials or procedures that do not meet the system design and the standards and codes, specified in this document.

e. All equipment and materials furnished and installed under this section shall be guaranteed against defects in materials and workmanship for a period of at least twelve (12) months from the date of final acceptance by the Owner. The defective materials and/or equipment shall be repaired or replaced, at the Owner's discretion, with no additional cost to the Owner.

115-2.2 Concrete structures. Concrete shall be proportioned, placed, and cured per Item P-610, Concrete for Miscellaneous Structures. Cast-in-place concrete structures shall be as shown on the plans.

115-2.3 Precast concrete structures. Precast concrete structures shall be furnished by a plant meeting National Precast Concrete Association Plant Certification Program or another engineer approved third party certification program. Provide precast concrete structures where shown on the plans.

Precast concrete structures shall be an approved standard design of the manufacturer. Precast units shall have mortar or bitumastic sealer placed between all joints to make them watertight. The structure shall be designed to withstand 100,000 lb. aircraft wheel loads, unless otherwise shown on the plans. Openings or knockouts shall be provided in the structure as detailed on the plans.

Threaded inserts and pulling eyes shall be cast in as shown on the plans.

If the Contractor chooses to propose a different structural design, signed and sealed shop drawings, design calculations, and other information requested by the RPR shall be submitted by the Contractor to allow for a full evaluation by the RPR. The RPR shall review per the process defined in the General Provisions.

115-2.4 Junction boxes. Junction boxes shall be L-867 Class 1 (non-load bearing) or L-868 Class 1 (load bearing) airport light bases that are encased in concrete. The light bases shall have a L-894 blank cover, gasket, and stainless steel hardware. All bolts, studs, nuts, lock washers, and other similar fasteners used for the light fixture assemblies must be fabricated from 316L (equivalent to EN 1.4404), 18-8, 410, or 416 stainless steel. If 18-8, 410, or 416 stainless steel is utilized it shall be passivated and be free from any discoloration. Covers shall be 3/8-inch thickness for L-867 and 3/4-inch thickness for L-868. All junction boxes shall be provided with both internal and external ground lugs.

115-2.5 Mortar. The mortar shall be composed of one part of cement and two parts of mortar sand, by volume. The cement shall be per the requirements in ASTM C150, Type I. The sand shall be per the requirements in ASTM C144. Hydrated lime may be added to the mixture of sand and cement in an amount not to exceed 15% of the weight of cement used. The hydrated lime shall meet the requirements of ASTM C206. Water shall be potable, reasonably clean and free of oil, salt, acid, alkali, sugar, vegetable, or other substances injurious to the finished product.

115-2.6 Concrete. Concrete shall be proportioned, placed, and cured per Item P-610, Concrete for Miscellaneous Structures.

115-2.7 Frames and covers. The frames shall conform to one of the following requirements:

- | | |
|-----------------------|--|
| a. ASTM A48 | Gray iron castings |
| b. ASTM A47 | Malleable iron castings |
| c. ASTM A27 | Steel castings |
| d. ASTM A283, Grade D | Structural steel for grates and frames |
| e. ASTM A536 | Ductile iron castings |
| f. ASTM A897 | Austempered ductile iron castings |

All castings specified shall withstand a maximum tire pressure of 250 psi and maximum load of 100,000 lbs.

All castings or structural steel units shall conform to the dimensions shown on the plans and shall be designed to support the loadings specified.

Each frame and cover unit shall be provided with fastening members to prevent it from being dislodged by traffic, but which will allow easy removal for access to the structure.

All castings shall be thoroughly cleaned. After fabrication, structural steel units shall be galvanized to meet the requirements of ASTM A123.

Each cover shall have the word "ELECTRIC" or other approved designation cast on it. Each frame and cover shall be as shown on the plans or approved equivalent. No cable notches are required.

Each manhole shall be provided with a "DANGER -- PERMIT-REQUIRED CONFINED SPACE, DO NOT ENTER" safety warning sign as detailed in the Contract Documents and in accordance with OSHA 1910.146 (c)(2).

115-2.8 Ladders. Ladders, if specified, shall be galvanized steel or as shown on the plans.

115-2.9 Reinforcing steel. All reinforcing steel shall be deformed bars of new billet steel meeting the requirements of ASTM A615, Grade 60.

115-2.10 Bedding/special backfill. Bedding or special backfill shall be as shown on the plans.

115-2.11 Flowable backfill. Flowable material used to backfill shall conform to the requirements of Item P-153, Controlled Low Strength Material.

115-2.12 Cable trays. Cable trays shall be of galvanized steel. Cable trays shall be located as shown on the plans.

115-2.13 Plastic conduit. Plastic conduit shall comply with Item L-110, Airport Underground Electrical Duct Banks and Conduits.

115-2.14 Conduit terminators. Conduit terminators shall be pre-manufactured for the specific purpose and sized as required or as shown on the plans.

115-2.15 Pulling-in irons. Pulling-in irons shall be manufactured with 7/8-inch diameter hot-dipped galvanized steel or stress-relieved carbon steel roping designed for concrete applications (7 strand, 1/2-inch diameter with an ultimate strength of 270,000 psi). Where stress-relieved carbon steel roping is used, a rustproof sleeve shall be installed at the hooking point and all exposed surfaces shall be encapsulated with a polyester coating to prevent corrosion.

115-2.16 Ground rods. Ground rods shall be one piece, copper or copper clad steel. The ground rods shall be of the length and diameter specified on the plans, but in no case shall they be less than 8 feet long nor less than 5/8 inch in diameter.

CONSTRUCTION METHODS

115-3.1 Unclassified excavation. It is the Contractor's responsibility to locate existing utilities within the work area prior to excavation. Damage to utility lines, through lack of care in excavating, shall be repaired or replaced to the satisfaction of the RPR without additional expense to the Owner.

The Contractor shall perform excavation for structures and structure footings to the lines and grades or elevations shown on the plans or as staked by the RPR. The excavation shall be of sufficient size to permit the placing of the full width and length of the structure or structure footings shown.

All excavation shall be unclassified and shall be considered incidental to Item L-115. Dewatering necessary for structure installation and erosion per federal, state, and local requirements is incidental to Item L-115.

Boulders, logs and all other objectionable material encountered in excavation shall be removed. All rock and other hard foundation material shall be cleaned of all loose material and cut to a firm surface either level, stepped or serrated, as directed by the RPR. All seams, crevices, disintegrated rock and thin strata shall be removed. When concrete is to rest on a surface other than rock, special care shall be taken not to disturb the bottom of the excavation. Excavation to final grade shall not be made until just before the concrete or reinforcing is to be placed.

The Contractor shall provide all bracing, sheeting and shoring necessary to implement and protect the excavation and the structure as required for safety or conformance to governing laws. The cost of bracing, sheeting and shoring shall be included in the unit price bid for the structure.

Unless otherwise provided, bracing, sheeting and shoring involved in the construction of this item shall be removed by the Contractor after the completion of the structure. Removal shall be effected in a manner that will not disturb or mar finished masonry. The cost of removal shall be included in the unit price bid for the structure.

After each excavation is completed, the Contractor shall notify the RPR. Structures shall be placed after the RPR has approved the depth of the excavation and the suitability of the foundation material.

Prior to installation the Contractor shall provide a minimum of 6 inches (150 mm) of sand or a material approved by the RPR as a suitable base to receive the structure. The base material shall be compacted and graded level and at proper elevation to receive the structure in proper relation to the conduit grade or ground cover requirements, as indicated on the plans.

115-3.2 Concrete structures. Concrete structures shall be built on prepared foundations conforming to the dimensions and form indicated on the plans. The concrete and construction methods shall conform to the requirements specified in Item P-610. Any reinforcement required shall be placed as indicated on the plans and shall be approved by the RPR before the concrete is placed.

115-3.3 Precast unit installations. Precast units shall be installed plumb and true. Joints shall be made watertight by use of sealant at each tongue-and-groove joint and at roof of manhole. Excess sealant shall be removed and severe surface projections on exterior of neck shall be removed.

115-3.4 Placement and treatment of castings, frames and fittings. All castings, frames and fittings shall be placed in the positions indicated on the Plans or as directed by the RPR and shall be set true to line and to correct elevation. If frames or fittings are to be set in concrete or cement mortar, all anchors or bolts shall be in place and position before the concrete or mortar is placed. The unit shall not be disturbed until the mortar or concrete has set.

Field connections shall be made with bolts, unless indicated otherwise. Welding will not be permitted unless shown otherwise on the approved shop drawings and written approval is granted by the casting manufacturer. Erection equipment shall be suitable and safe for the workman. Errors in shop fabrication or deformation resulting from handling and transportation that prevent the proper assembly and fitting of parts shall be reported immediately to the RPR and approval of the method of correction shall be obtained. Approved corrections shall be made at Contractor's expense.

Anchor bolts and anchors shall be properly located and built into connection work. Bolts and anchors shall be preset by the use of templates or such other methods as may be required to locate the anchors and anchor bolts accurately.

Pulling-in irons shall be located opposite all conduit entrances into structures to provide a strong, convenient attachment for pulling-in blocks when installing cables. Pulling-in irons shall be set directly into the concrete walls of the structure.

115-3.5 Installation of ladders. ~~Ladders shall be installed such that they may be removed if necessary. Mounting brackets shall be supplied top and bottom and shall be cast in place during fabrication of the structure or drilled and grouted in place after erection of the structure.~~

115-3.6 Removal of sheeting and bracing. In general, all sheeting and bracing used to support the sides of trenches or other open excavations shall be withdrawn as the trenches or other open excavations are being refilled. That portion of the sheeting extending below the top of a structure shall be withdrawn, unless otherwise directed, before more than 6 inches of material is placed above the top of the structure and before any bracing is removed. Voids left by the sheeting shall be carefully refilled with selected material and rammed tight with tools especially adapted for the purpose or otherwise as may be approved.

The RPR may direct the Contractor to delay the removal of sheeting and bracing if, in his judgment, the installed work has not attained the necessary strength to permit placing of backfill.

115-3.7 Backfilling. After a structure has been completed, the area around it shall be backfilled in horizontal layers not to exceed 6 inches in thickness measured after compaction to the density requirements in Item P-152. Each layer shall be deposited all around the structure to approximately the same elevation. The top of the fill shall meet the elevation shown on the plans or as directed by the RPR.

Backfill shall not be placed against any structure until approval is given by the RPR. In the case of concrete, such approval shall not be given until tests made by the laboratory under supervision of the RPR establish that the concrete has attained sufficient strength to provide a factor of safety against damage or strain in withstanding any pressure created by the backfill or the methods used in placing it.

Where required, the RPR may direct the Contractor to add, at his own expense, sufficient water during compaction to assure a complete consolidation of the backfill. The Contractor shall be responsible for all damage or injury done to conduits, duct banks, structures, property or persons due to improper placing or compacting of backfill.

115-3.8 Connection of duct banks. To relieve stress of joint between concrete-encased duct banks and structure walls, reinforcement rods shall be placed in the structure wall and shall be formed and tied into duct bank reinforcement at the time the duct bank is installed.

115-3.9 Grounding. A ground rod shall be installed in the floor of all concrete structures so that the top of rod extends 6 inches above the floor. The ground rod shall be installed within one foot of a corner of the concrete structure. Ground rods shall be installed prior to casting the bottom slab. Where the soil condition does not permit driving the ground rod into the earth without damage to the ground rod, the Contractor shall drill a 4-inch diameter hole into the earth to receive the ground rod. The hole around the ground rod shall be filled throughout its length, below slab, with Portland cement grout. Ground rods shall be installed in precast bottom slab of structures by drilling a hole through bottom slab and installing the ground rod. Bottom slab penetration shall be sealed watertight with Portland cement grout around the ground rod.

A grounding bus of 4/0 bare stranded copper shall be exothermically bonded to the ground rod and loop the concrete structure walls. The ground bus shall be a minimum of one foot above the floor of the structure and separate from other cables. No. 2 American wire gauge (AWG) bare copper pigtailed shall bond the grounding bus to all cable trays and other metal hardware within the concrete structure. Connections to the grounding bus shall be exothermic. If an exothermic weld is not possible, connections to the grounding bus shall be made by using connectors approved for direct burial in soil or concrete per UL 467. Hardware connections may be mechanical, using a lug designed for that purpose.

115-3.10 Cleanup and repair. After erection of all galvanized items, damaged areas shall be repaired by applying a liquid cold-galvanizing compound per MIL-P-21035. Surfaces shall be prepared and compound applied per the manufacturer's recommendations.

Prior to acceptance, the entire structure shall be cleaned of all dirt and debris.

115-3.11 Restoration. After the backfill is completed, the Contractor shall dispose of all surplus material, dirt and rubbish from the site. The Contractor shall restore all disturbed areas equivalent to or better than their original condition. All sodding, grading and restoration shall be considered incidental to the respective Item L-115 pay item.

The Contractor shall grade around structures as required to provide positive drainage away from the structure.

Areas with special surface treatment, such as roads, sidewalks, or other paved areas shall have backfill compacted to match surrounding areas, and surfaces shall be repaired using materials comparable to original materials.

Following restoration of all trenching near airport movement surfaces, the Contractor shall thoroughly visually inspect the area for foreign object debris (FOD), and remove any such FOD that is found. This FOD inspection and removal shall be considered incidental to the pay item of which it is a component part.

After all work is completed, the Contractor shall remove all tools and other equipment, leaving the entire site free, clear and in good condition.

115-3.12 Inspection. Prior to final approval, the electrical structures shall be thoroughly inspected for conformance with the plans and this specification. Any indication of defects in materials or workmanship shall be further investigated and corrected. The earth resistance to ground of each ground rod shall not exceed 25 ohms. Each ground rod shall be tested using the fall-of-potential ground impedance test per American National Standards Institute / Institute of Electrical and Electronic Engineers (ANSI/IEEE) Standard 81. This test shall be performed prior to establishing connections to other ground electrodes.

~~115-3.13 Manhole elevation adjustments. The Contractor shall adjust the tops of existing manholes in areas designated in the Contract Documents to the new elevations shown. The Contractor shall be responsible for determining the exact height adjustment required to raise or lower the top of each manhole to the new elevations. The existing top elevation of each manhole to be adjusted shall be determined in the field and subtracted/added from the proposed top elevation.~~

~~The Contractor shall remove/extend the existing top section or ring and cover on the manhole structure or manhole access. The Contractor shall install precast concrete sections or grade rings of the required dimensions to adjust the manhole top to the new proposed elevation or shall cut the existing manhole walls to shorten the existing structure, as required by final grades. The Contractor shall reinstall the manhole top section or ring and cover on top and check the new top elevation.~~

~~The Contractor shall construct a concrete slab around the top of adjusted structures located in graded areas that are not to be paved. The concrete slab shall conform to the dimensions shown on the plans.~~

~~115-3.14 Duct extension to existing ducts. Where existing concrete encased ducts are to be extended, the duct extension shall be concrete encased plastic conduit. The fittings to connect the ducts together shall be standard manufactured connectors designed and approved for the purpose. The duct extensions shall be installed according to the concrete encased duct detail and as shown on the plans.~~

METHOD OF MEASUREMENT

115-4.1 Electrical manholes and junction structures shall be measured by each unit completed in place and accepted. The following items shall be included in the price of each unit: All required excavation and dewatering; sheeting and bracing; all required backfilling with on-site materials; restoration of all surfaces and finished grading and turfing; all required connections; temporary cables and connections; and ground rod testing

115-4.2 Airfield Handhole Cover Replacements shall be measured by the completed unit installed, in place, completed, and accepted. Separate measurement shall not be made for the various types and sizes.

BASIS OF PAYMENT

115-5.1 The accepted quantity of electrical manholes and junction structures will be paid for at the Contract unit price per each, complete and in place. This price shall be full compensation for furnishing all materials and for all preparation, excavation, backfilling and placing of the materials, furnishing and installation of appurtenances and connections to duct banks and other structures as may be required to complete the item as shown on the plans and for all labor, equipment, tools and incidentals necessary to complete the structure.

115-5.2 Payment shall be made by the contract unit price for Airfield Handhole Cover Replacement, per the type and dimension installed, complete and in-place. This price shall be full compensation for furnishing all materials and for all preparation, assembly, and installation of these materials, and for all labor, equipment, tools, and incidentals necessary, including but not limited to, spacers, concrete, rebar, dewatering, excavating, backfill, topsoil, sodding and pavement restoration, where required, to complete this item as shown in the plans and to the satisfaction of the RPR.

Payment will be made under:

Item L-115-5.1	Electrical Junction Structure, L-867 Class 1, Size 16" Diameter by 24" Depth - Per Each
Item L-115-5.2	Electrical Junction Structure Plaza, Two -867 Class 1, Size 16" Diameter by 24" Depth Cans – Per Each
Item L-115-5.3	3'x 4' Airfield Handhole Cover Replacement – Per Each

REFERENCES

The publications listed below form a part of this specification to the extent referenced. The publications are referred to within the text by the basic designation only.

American National Standards Institute / Insulated Cable Engineers Association (ANSI/ICEA)

ANSI/IEEE STD 81 IEEE Guide for Measuring Earth Resistivity, Ground Impedance, and Earth Surface Potentials of a Ground System

Advisory Circular (AC)

AC 150/5345-7 Specification for L-824 Underground Electrical Cable for Airport Lighting Circuits

AC 150/5345-26 Specification for L-823 Plug and Receptacle, Cable Connectors

AC 150/5345-42 Specification for Airport Light Bases, Transformer Housings, Junction Boxes, and Accessories

AC 150/5340-30 Design and Installation Details for Airport Visual Aids

AC 150/5345-53 Airport Lighting Equipment Certification Program

Commercial Item Description (CID)

A-A 59544 Cable and Wire, Electrical (Power, Fixed Installation)

ASTM International (ASTM)

ASTM A27 Standard Specification for Steel Castings, Carbon, for General Application

ASTM A47 Standard Specification for Ferritic Malleable Iron Castings

ASTM A48 Standard Specification for Gray Iron Castings

ASTM A123 Standard Specification for Zinc (Hot Dip Galvanized) Coatings on Iron and Steel Products

ASTM A283 Standard Specification for Low and Intermediate Tensile Strength Carbon Steel Plates

ASTM A536 Standard Specification for Ductile Iron Castings

ASTM A615 Standard Specification for Deformed and Plain Carbon-Steel Bars for Concrete Reinforcement

ASTM A897 Standard Specification for Austempered Ductile Iron Castings

ASTM C144 Standard Specification for Aggregate for Masonry Mortar

ASTM C150 Standard Specification for Portland Cement

ASTM C206 Standard Specification for Finishing Hydrated Lime

FAA Engineering Brief (EB)

EB #83 In Pavement Light Fixture Bolts

Mil Spec

MIL-P-21035 Paint High Zinc Dust Content, Galvanizing Repair

National Fire Protection Association (NFPA)

NFPA-70

National Electrical Code (NEC)

END OF ITEM L-115

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CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) - PAGE 1 OF 2

1. COORDINATION
 - A. CONTRACTOR PROGRESS MEETINGS - THE OWNER, ENGINEER AND CONTRACTOR WILL HOLD PROGRESS MEETINGS ON A COORDINATED SCHEDULE DURING CONSTRUCTION. OPERATIONAL SAFETY WILL BE A STANDING AGENDA ITEM IN SUCH MEETINGS.
 - B. SCOPE OR SCHEDULE CHANGES - THE OWNER AND/OR ENGINEER WILL CALL SUCH COORDINATION CONFERENCES AS MAY SEEM EXPEDIENT TO HIM FOR THE PURPOSE OF ASSURING COORDINATION OF THE WORK COVERED BY THIS CONTRACT AND/OR SCOPE OR SCHEDULE CHANGES. THE CONTRACTOR SHALL ATTEND ALL SUCH CONFERENCES.
 - C. FAA ATO COORDINATION - COORDINATION WITH THE FAA ATO FOR THE REQUIRED SHUTDOWN OF FAA FACILITIES FOR THIS PROJECT HAS BEEN COMPLETED. THE FAA ATO CONTACT FOR THIS PROJECT IS BRUCE HYERS, (615) 717-8592, FAA LAKE CHARLES TECH OPS MANAGER.
2. PHASING

DURING PERFORMANCE OF THIS PROJECT, THE AIRPORT RUNWAYS, TAXIWAYS, AND AIRCRAFT PARKING APRONS SHALL REMAIN IN USE BY AIRCRAFT TO THE MAXIMUM EXTENT POSSIBLE. THE PROJECT SHALL BE PHASED TO REDUCE OPERATIONAL IMPACTS AT THE AIRPORT.

 - A. PHASE ELEMENTS - IF NECESSARY FOR A GIVEN PHASE, EACH PHASE OF THE CONSTRUCTION SAFETY DRAWINGS SHALL DETAIL THE AREAS CLOSED TO AIRCRAFT OPERATIONS, ESTIMATED DURATION OF CLOSURES, TAXI ROUTES, ARFF ACCESS ROUTES, CONSTRUCTION STAGING AREAS, CONSTRUCTION ACCESS AND HAUL ROUTES, NAVAID IMPACTS, LIGHTING AND MARKING CHANGES, AVAILABLE RUNWAY LENGTH, DECLARED DISTANCES, HAZARD MARKING AND LIGHTING, AND REQUIRED LEAD TIME FOR NOTAMS.
 - B. CONSTRUCTION SAFETY AND PHASING DRAWINGS - SEE SHEET GC100 THROUGH GC503 FOR CONSTRUCTION SAFETY AND PHASING DRAWINGS.
3. AREAS OF OPERATIONS AFFECTED BY CONSTRUCTION ACTIVITY
 - A. IDENTIFICATION OF AFFECTED AREAS - SEE "AIRFIELD AREAS OF OPERATION AFFECTED BY CONSTRUCTION" TABLE AND CONSTRUCTION SAFETY DRAWINGS FOR AIRFIELD AREAS OF OPERATIONS AFFECTED BY CONSTRUCTION.
 - B. MITIGATION EFFORTS - SEE TABLE ON SHEET GC002 OF THE DRAWINGS FOR MITIGATION EFFORTS OF OPERATIONS AFFECTED BY CONSTRUCTION.
4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

THE CONTRACTOR MUST NOT CONDUCT ANY CONSTRUCTION ACTIVITY WITHIN NAVIGATIONAL AID RESTRICTED AREAS WITHOUT PRIOR APPROVAL FROM THE LOCAL FAA AIRWAY FACILITIES SECTOR REPRESENTATIVE. NAVIGATIONAL AIDS INCLUDE INSTRUMENT LANDING SYSTEM COMPONENTS, VERY HIGH-FREQUENCY OMNI-DIRECTIONAL RANGE STATIONS, AND AIRPORT SURVEILLANCE RADAR. SUCH RESTRICTED AREAS ARE DEPICTED ON CONSTRUCTION PLANS. PLANNED CONSTRUCTION ACTIVITIES WILL IMPACTS ON THE FUNCTIONALITY AND SERVICEABILITY OF THE NAVAIDS.
5. CONTRACTOR ACCESS
 - A. LOCATION OF STOCKPILED MATERIALS - THE CONTRACTOR SHALL INSTALL A TEMPORARY FENCE AROUND HIS CONSTRUCTION STAGING AREA TO SEPARATE HIS MATERIAL STOCKPILE, EQUIPMENT STORAGE, AND PARKING AREAS FROM THE PUBLIC. NO PERSONAL VEHICLES OF CONTRACTOR'S EMPLOYEES WILL BE ALLOWED INSIDE THE SECURED AREA OF THE AIRPORT. ALL MATERIAL DELIVERIES SHALL BE RECEIVED IN THE STAGING AREA RESERVED BY THE CONTRACTOR. NO DELIVERY TRUCKS WILL BE ALLOWED ACCESS TO A SECURED AREA OF THE AIRPORT BEYOND THIS STAGING AREA. STOCKPILED MATERIALS AND EQUIPMENT ARE NOT PERMITTED WITHIN THE ACTIVE RUNWAY SAFETY AREA AND OBSTACLE FREE ZONE. THE CONTRACTOR SHALL RECEIVE APPROVAL FROM THE ENGINEER AND FAA AIR SPACING OFFICE PRIOR TO LOCATING STOCKPILES OR EQUIPMENT WITHIN THE OBJECT FREE AREA, SAFETY AREA, OR OBSTACLE FREE ZONE. NO STOCKPILE SHALL BE GREATER THAN 15-FT IN HEIGHT.
 - B. VEHICLE AND PEDESTRIAN OPERATIONS - SEE THE CONSTRUCTION SAFETY DRAWINGS FOR CONSTRUCTION SITE PARKING, EQUIPMENT STORAGE AREAS, AND ACCESS AND HAUL ROUTES. VEHICULAR TRAFFIC SHALL ALWAYS YIELD TO AIRCRAFT TRAFFIC.

WHEN ANY VEHICLE, OTHER THAN ONE THAT HAS PRIOR APPROVAL FROM THE AIRPORT OPERATOR, MUST TRAVEL OVER ANY PORTION OF AN AIRCRAFT MOVEMENT AREA, IT WILL BE ESCORTED AND PROPERLY IDENTIFIED. TO OPERATE IN THOSE AREAS DURING DAYLIGHT HOURS, THE VEHICLE MUST HAVE A FLAG OR BEACON ATTACHED TO IT. ANY VEHICLE OPERATING ON THE MOVEMENT AREAS DURING HOURS OF DARKNESS OR REDUCED VISIBILITY MUST BE EQUIPPED WITH A FLASHING DOME-TYPE LIGHT, THE COLOR OF WHICH IS IN ACCORDANCE WITH LOCAL OR STATE CODES.

ALL CONSTRUCTION VEHICLES SHALL BE CLEARLY IDENTIFIED FOR CONTROL PURPOSES BY PROMINENTLY DISPLAYING THE COMPANY NAME ON EACH SIDE OF THE VEHICLE. THE IDENTIFICATION SYMBOLS SHOULD BE A MINIMUM 8-INCH BLOCK-TYPE CHARACTERS OF A CONTRASTING COLOR AND EASY TO READ. THEY MAY BE APPLIED EITHER BY USING TAPE OR A WATER-SOLUBLE PAINT TO FACILITATE REMOVAL. MAGNETIC SIGNS ARE ALSO ACCEPTABLE. IN ADDITION, VEHICLES MUST DISPLAY IDENTIFICATION MEDIA.

AT 14 CFR PART 139 CERTIFIED AND TOWERED AIRPORTS, ALL VEHICLE OPERATORS HAVING ACCESS TO THE MOVEMENT AREA SHALL FOLLOW AIRPORT PROCEDURES FOR THE OPERATION OF GROUND VEHICLES AND THE CONSEQUENCES OF NONCOMPLIANCE OR BE ESCORTED BY SOMEONE WHO IS.

 - C. CONTROL OF GATES - THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE SECURITY OF THE ACCESS GATES BY KEEPING THE ACCESS GATE LOCKED OR GUARDED AT ALL TIMES. SHOULD THE CONTRACTOR FAIL, AT ANY TIME, TO KEEP THE ACCESS GATE LOCKED OR GUARDED, THERE SHALL BE A FINE OF \$200.00 ASSESSED TO THE CONTRACTOR. FOR EACH OCCURRENCE THAT THE CONTRACTOR FAILS TO MAINTAIN THE SECURITY OF THE ACCESS GATE. ALL FINES ASSESSED TO THE CONTRACTOR SHALL BE DEDUCTED FROM ANY MONIES DUE TO HIM/HER.
6. WILDLIFE MANAGEMENT

IF APPLICABLE, THE CONTRACTOR SHALL REVIEW AND ADHERE TO THE CONTENTS OF THE AIRPORT OPERATOR'S WILDLIFE HAZARD MANAGEMENT PLAN. THE CONTRACTOR SHALL ALSO REVIEW AC 150/5200-33, HAZARDOUS WILDLIFE ATTRACTANTS ON OR NEAR AIRPORTS, AND CERTALERT 98-05, GRASSES ATTRACTIVE TO HAZARDOUS WILDLIFE (www.faa.gov). THE CONTRACTOR SHALL CAREFULLY CONTROL AND CONTINUOUSLY REMOVE WASTE OR LOOSE MATERIALS THAT MIGHT ATTRACT WILDLIFE. CONTRACTOR PERSONNEL MUST BE AWARE OF AND AVOID CONSTRUCTION ACTIVITIES THAT CAN CREATE WILDLIFE HAZARDS ON AIRPORTS. THE CONTRACTOR SHALL MITIGATE THE FOLLOWING ITEMS:

 - A. TRASH - THE CONTRACTOR SHALL PERFORM TRASH CLEAN-UP ON A DAILY BASIS.
 - B. STANDING WATER - THE CONTRACTOR SHALL PROVIDE TEMPORARY DRAINAGE DURING CONSTRUCTION TO AVOID STANDING WATER.
 - C. TALL GRASS AND SEEDS - THE CONTRACTOR SHALL ADHERE TO THE REQUIREMENTS OF SECTION T-901, SEEDING OF THE CONTRACT DOCUMENTS AND SPECIFICATIONS.
 - D. POORLY MAINTAINED FENCING AND GATES - THE CONTRACTOR SHALL IMMEDIATELY REPORT ANY DAMAGE TO GATES OR FENCES. THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRS TO ANY GATES OR FENCES CAUSED BY NEGLIGENCE BY THE CONTRACTOR.
 - E. DISRUPTION OF EXISTING WILDLIFE HABITAT - THE CONTRACTOR SHALL NOTIFY THE AIRPORT IMMEDIATELY OF ANY WILDLIFE SIGHTINGS.
7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

THE CONTRACTOR SHALL ENSURE THAT THE PAVEMENT SURFACES ARE KEPT CLEAN FROM DIRT, MUD, AND OTHER DEBRIS FROM THE CONTRACTOR'S EQUIPMENT. FREQUENT CLEAN UP IN THE VICINITY OF CONTRACTOR'S WORK AREAS IS REQUIRED. SEE AC 150/5210-24, FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT (www.faa.gov) FOR FURTHER INSTRUCTION.



8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

IF ANY CONSTRUCTION VEHICLE OR EQUIPMENT IS OPERATED WITHIN AIRPORT PROPERTY, THE CONTRACTOR MUST BE ADEQUATELY PREPARED TO EXPEDITIOUSLY CONTAIN AND CLEAN-UP SPILLS RESULTING FROM FUEL OR HYDRAULIC FLUID LEAKS. SPECIAL CARE MUST ALSO BE TAKEN WHEN HANDLING OR TRANSPORTING HAZARDOUS MATERIALS ON AIRPORT PROPERTY. SEE AC 150/5320-15, MANAGEMENT OF AIRPORT INDUSTRIAL WASTE (www.faa.gov), FOR FURTHER INSTRUCTION.
9. NOTIFICATION OF CONSTRUCTION ACTIVITIES
 - A. LIST OF RESPONSIBLE REPRESENTATIVES - A POINT OF CONTACT LIST WILL BE COMPLETED AS PART OF THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) AND WILL BE DELIVERED TO ALL PARTIES PRIOR TO CONSTRUCTION.
 - B. NOTICES TO AIRMEN (NOTAM) - BEFORE BEGINNING ANY CONSTRUCTION ACTIVITY, THE CONTRACTOR MUST, THROUGH THE AIRPORT OPERATOR, GIVE NOTICE USING THE NOTAM SYSTEM OF PROPOSED LOCATION, TIME, AND DATE OF COMMENCEMENT OF CONSTRUCTION. UPON COMPLETION OF WORK AND RETURN OF ALL SUCH AREAS TO STANDARD CONDITIONS, THE CONTRACTOR MUST, THROUGH THE AIRPORT OPERATOR, VERIFY THE CANCELLATION OF ALL NOTICES ISSUED VIA THE NOTAM SYSTEM.
 - C. EMERGENCY NOTIFICATION PROCEDURES - IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911, THEN NOTIFY THE ENGINEER AND AIRPORT MANAGER.
 - D. COORDINATION WITH ARFF PERSONNEL - ANY DEACTIVATION OF WATER LINES OR HYDRANTS, REROUTING OF ACCESS ROUTES, OR USE OF HAZARDOUS MATERIALS ON THE AIRFIELD SHALL BE COORDINATED AND APPROVED BY THE AIRPORT'S ARFF PERSONNEL PRIOR TO EXECUTION OF SUCH ACTIVITIES.
 - E. NOTIFICATION TO THE FAA - THE CONTRACTOR SHALL ENSURE, THROUGH THE ENGINEER, THAT ALL CONSTRUCTION EQUIPMENT OVER 15 FT IN HEIGHT IS AIR SPACED THROUGH THE APPROPRIATE FAA REGIONAL OR DISTRICT OFFICE PRIOR TO USING SUCH EQUIPMENT ON SITE.
 - F. SHUTDOWN OF ANY NAVAID (AIRPORT OR FAA OWNED) SHALL BE COORDINATED WITH THE FAA ATO 45 DAYS PRIOR TO THE PROPOSED SHUTDOWN. THE CONTRACTOR SHALL PROVIDE AN ADDITIONAL SEVEN DAYS ADVANCE NOTICE TO THE AIRPORT TO COORDINATE WITH THE FAA ATO TECH OPS OFFICE RESPONSIBLE FOR THE FAA FACILITIES. SHUTDOWN OF AN AIRPORT OWNED AND FAA MAINTAINED NAVAID OF 24 HOURS OR GREATER, OR MORE THAN 4 HOURS DAILY ON CONSECUTIVE DAYS, SHALL BE COORDINATED WITH THE FAA ATO A MINIMUM OF 45 DAYS PRIOR TO THE SHUTDOWN.
10. INSPECTION REQUIREMENTS
 - A. DAILY INSPECTIONS - THE CONTRACTOR SHALL PERFORM DAILY SAFETY INSPECTIONS TO VERIFY ALL CONSTRUCTION OPERATIONS ARE IN CONFORMANCE WITH THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP).
 - B. INTERIM INSPECTIONS - PRIOR TO OPENING ANY PORTION OF THE AIRPORT TO TRAFFIC, THE CONTRACTOR, ENGINEER, AND AIRPORT OPERATOR SHALL PERFORM A SAFETY INSPECTION OF THE AREA TO BE OPENED TO TRAFFIC TO VERIFY CONFORMANCE WITH THE CSPP AND FAA STANDARDS.
 - C. FINAL INSPECTIONS - PRIOR TO OPENING ANY PORTION OF THE AIRPORT TO TRAFFIC, THE CONTRACTOR, ENGINEER, AND AIRPORT OPERATOR SHALL PERFORM A SAFETY INSPECTION OF THE AREA TO BE OPENED TO TRAFFIC TO VERIFY CONFORMANCE WITH THE CSPP AND FAA STANDARDS.
11. UNDERGROUND UTILITIES

UNDERGROUND UTILITIES EXIST WITHIN AND ADJACENT TO THE LIMITS OF CONSTRUCTION. AN ATTEMPT HAS BEEN MADE TO LOCATE THESE UTILITIES ON THE PLANS. HOWEVER, ALL EXISTING UTILITIES MAY NOT BE SHOWN AND THE ACTUAL LOCATIONS OF THE UTILITIES MAY VARY FROM THE LOCATIONS SHOWN. PRIOR TO BEGINNING ANY TYPE OF EXCAVATION, THE CONTRACTOR SHALL CONTACT THE UTILITIES INVOLVED AND MAKE ARRANGEMENTS FOR THE LOCATION OF THE UTILITIES ON THE GROUND. THE CONTRACTOR SHALL MAINTAIN THE UTILITY LOCATION MARKINGS UNTIL THEY ARE NO LONGER NECESSARY.

TEXAS STATE LAW, THE UNDERGROUND FACILITIES DAMAGE PREVENTION ACT, REQUIRES TWO WORKING DAYS ADVANCE NOTIFICATION THROUGH THE ONE-CALL SYSTEM CENTER BEFORE EXCAVATING USING MECHANIZED EQUIPMENT OR EXPLOSIVES (EXCEPT IN THE CASE OF AN EMERGENCY). THE ONE-CALL SYSTEM PHONE NUMBER IS 1-800-344-8377. THE CONTRACTOR IS ADVISED THAT THERE IS A SEVERE PENALTY FOR NOT MAKING THIS CALL. NOT ALL UTILITY COMPANIES ARE MEMBERS OF THE TEXAS ONE-CALL SYSTEM; THEREFORE, THE CONTRACTOR IS ADVISED TO CONTACT ALL NON-MEMBER UTILITIES AS WELL AS THE ONE-CALL SYSTEM.
12. PENALTIES

FAILURE OF THE CONTRACTOR (INCLUDING EMPLOYEES) OR ANY OF HIS SUBCONTRACTORS (INCLUDING EMPLOYEES) TO COMPLY WITH ATCT INSTRUCTIONS, THE AIRPORT SAFETY PLAN, OR ANY OF THE OTHER REQUIREMENTS OF THE AIRPORT WHILE OPERATING ON AIRPORT PROPERTY, SHALL BE SUBJECT TO THE FOLLOWING:

 - A. FIRST OFFENSE - THE CONTRACTOR SHALL RECEIVE A FINE OF \$1,000.00, AND THE VEHICLE OPERATOR WILL RECEIVE A LOSS OF DRIVING PRIVILEGES ON THE AIRPORT. IN ADDITION, ANY FINES OR PENALTIES IMPOSED ON THE AIRPORT AS A RESULT OF THE INCIDENT WILL BE ASSESSED TO THE CONTRACTOR.
 - B. SECOND OFFENSE - THE CONTRACTOR SHALL RECEIVE A FINE OF \$5,000.00 TO BE DEDUCTED FROM ANY MONIES DUE HIM, AND THE VEHICLE OPERATOR WILL RECEIVE A LOSS OF DRIVING PRIVILEGES ON THE AIRPORT. IN ADDITION, ANY FINE OR PENALTIES IMPOSED ON THE AIRPORT AS A RESULT OF THE INCIDENT WILL BE ASSESSED TO THE CONTRACTOR.
 - C. THIRD OFFENSE - WORK WILL BE SUSPENDED. THE CONTRACTOR (INCLUDING EMPLOYEES) AND ANY OF HIS SUBCONTRACTORS (INCLUDING EMPLOYEES) WHO WILL OPERATE GROUND VEHICLES ON THE AIRPORT SHALL SUCCESSFULLY COMPLETE, FOR A SECOND TIME, FORMALIZED AIRPORT SAFETY TRAINING, TO BE CONDUCTED BY AIRPORT STAFF. WHEN THE CONTRACTOR'S EMPLOYEES HAVE COMPLETED AIRPORT SAFETY TRAINING TO THE SATISFACTION OF THE OWNER, WORK MAY CONTINUE AT THE DISCRETION OF THE OWNER.
13. SPECIAL CONDITIONS

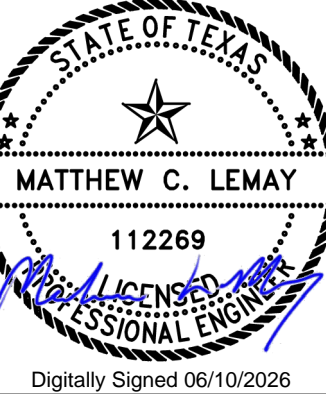
FAA MORATORIUM DATES -

 - MEMORIAL DAY - MAY 21, 2026 TO MAY 26, 2026
 - INDEPENDENCE DAY - JULY 1, 2026 TO JULY 6, 2026
 - LABOR DAY - SEPTEMBER 3, 2026 TO SEPTEMBER 8, 2026
 - THANKSGIVING - NOVEMBER 22, 2026 TO DECEMBER 1, 2026
 - CHRISTMAS/NEW YEAR - DECEMBER 19, 2026 TO JANUARY 4, 2027
 - MEMORIAL DAY - MAY 20, 2027 TO MAY 25, 2027
 - INDEPENDENCE DAY - JUNE 30, 2027 TO JULY 5, 2026



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REGISTRATION NO. F-5713



Digitally Signed 06/10/2026

REV.	DATE	DESCRIPTION	BY
1	6/10/26	ADDENDUM NO. 3	MCL



JACK BROOKS REGIONAL AIRPORT
JEFFERSON COUNTY, TEXAS

CONSTRUCTION SAFETY & PHASING NOTES 1

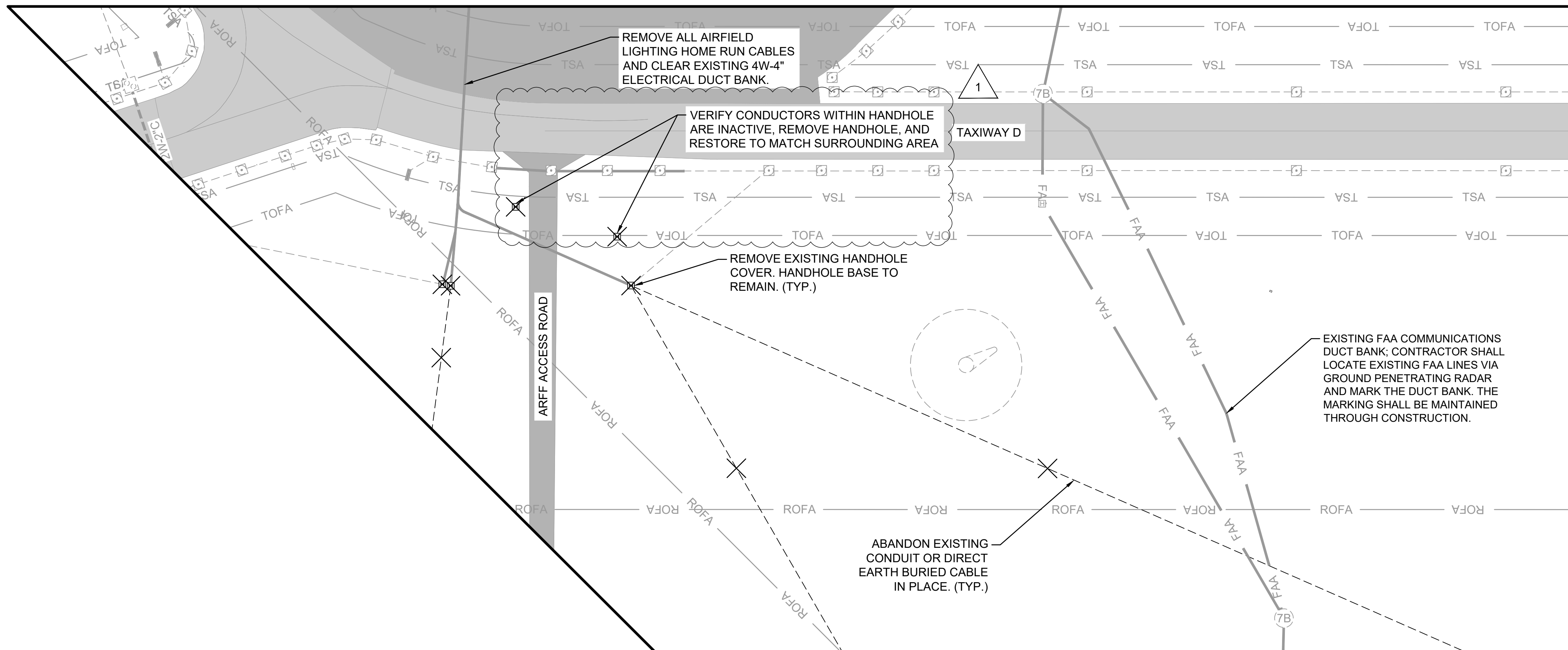
JOB NO.: 2400880
DATE: APR 2026
DESIGNED BY: DAG
DRAWN BY: DAG

BAR IS ONE INCH ON ORIGINAL DRAWING
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

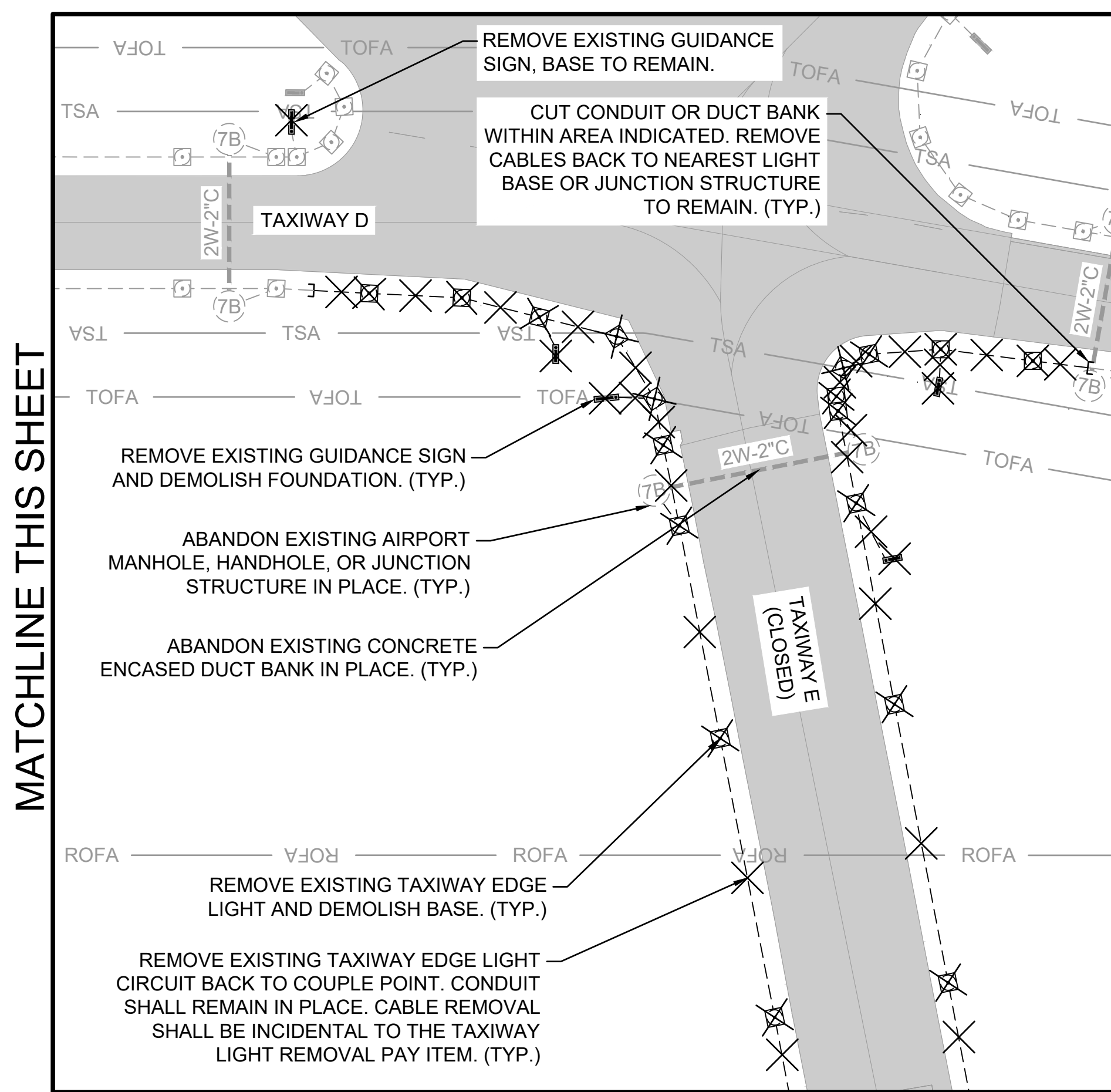
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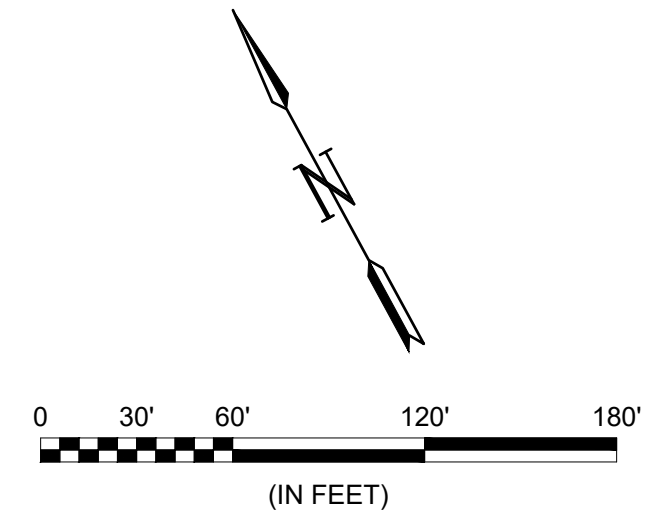
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MATCHLINE SHEET ED-103



MATCHLINE SHEET ED-103



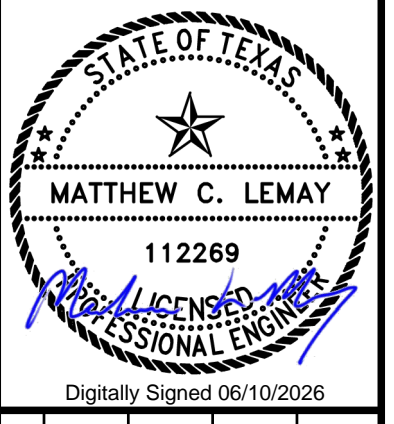
MATCHLINE THIS SHEET

- GENERAL NOTES:**
- SEE SHEET EN001 FOR ELECTRICAL DEMOLITION LEGEND, CONSTRUCTION NOTES, CAUTION NOTES, ELECTRICAL SAFETY NOTES, AND DEMOLITION NOTES.
 - SEE THE ED500 SERIES FOR LIGHTING REMOVAL DETAILS.
 - EXISTING CIRCUIT ROUTING IS APPROXIMATE. IF ROUTING DIFFERS IN THE FIELD, COORDINATE WITH ENGINEER.
 - CABLE REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO THE REMOVAL ITEMS.



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REV.	DATE	DESCRIPTION	BY
1	6/10/26	ADDENDUM NO. 3	MCL



JACK BROOKS REGIONAL AIRPORT
 JEFFERSON COUNTY, TEXAS

AIRFIELD LIGHTING REHABILITATION

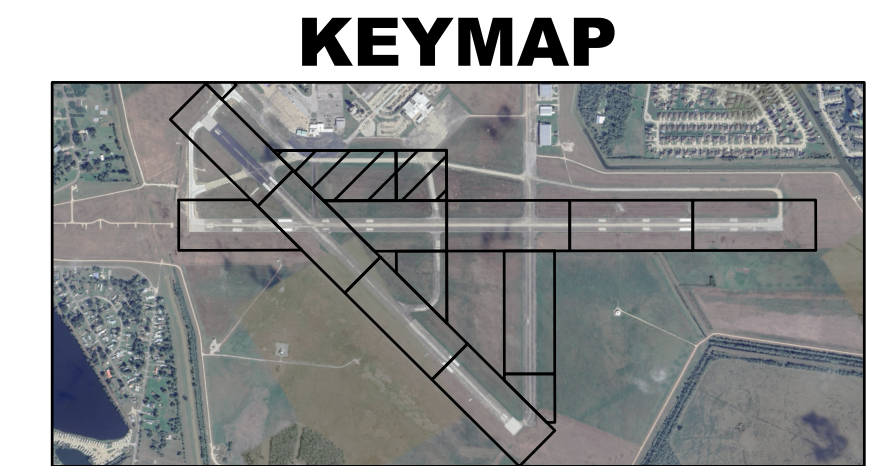
LIGHTING DEMOLITION PLAN 6

JOB NO.: 2400880
 DATE: APR 2026
 DESIGNED BY: MCL
 DRAWN BY: CCC

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DRAWING NUMBER
ED106

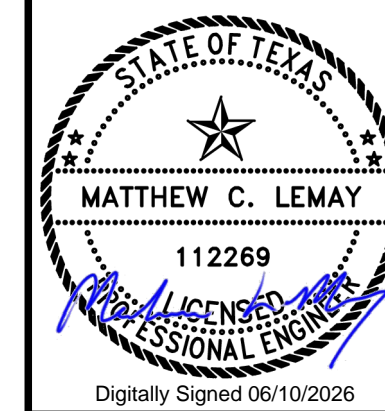
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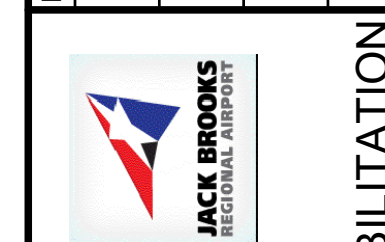
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REV.	DATE	DESCRIPTION	BY
1	6/10/26	ADDENDUM NO. 3	MCL



JACK BROOKS REGIONAL AIRPORT
JEFFERSON COUNTY, TEXAS

AIRFIELD LIGHTING REHABILITATION
LIGHTING INSTALLATION DETAILS 7

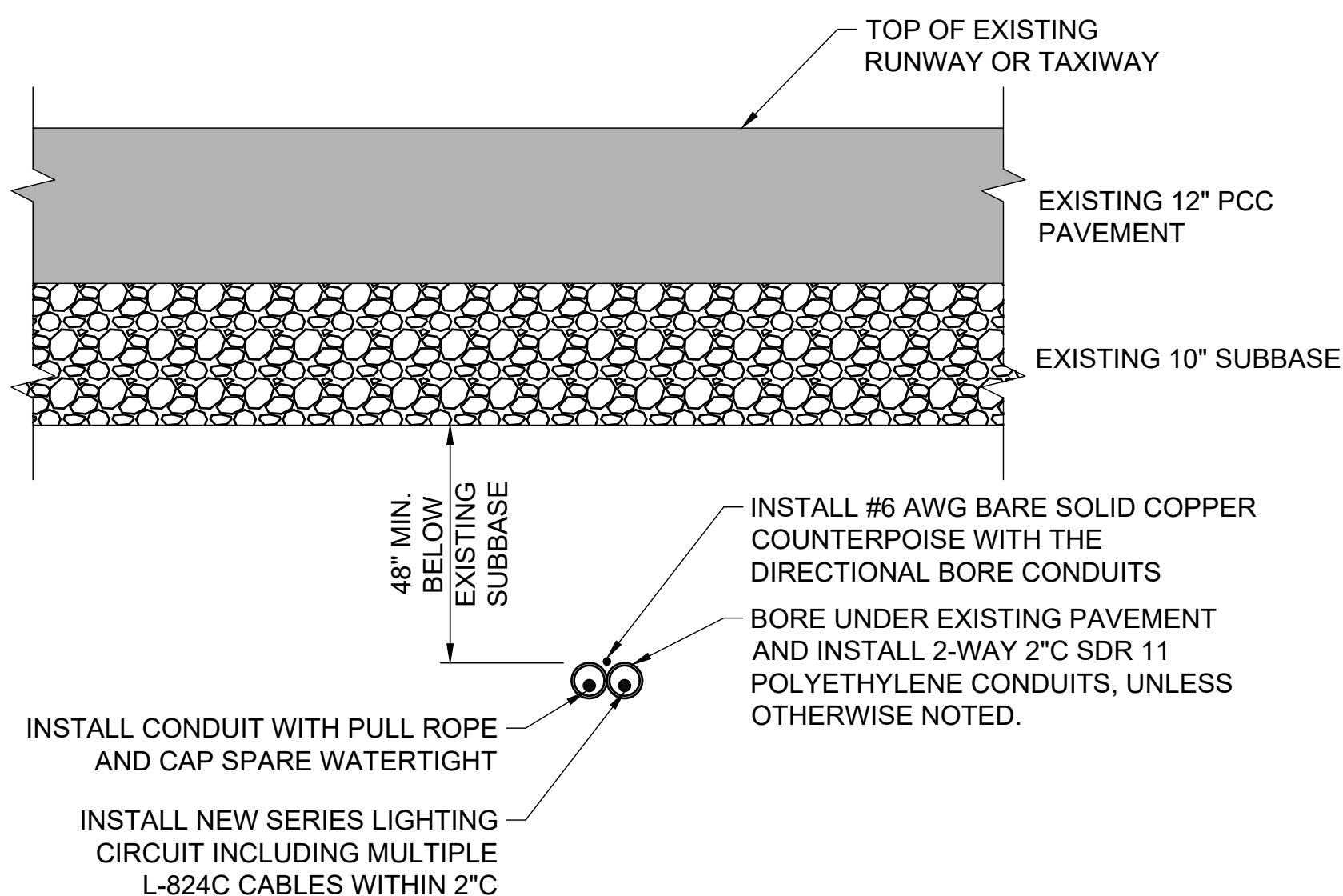
JOB NO.: 2400880
DATE: APR 2026
DESIGNED BY: MCL
DRAWN BY: CCC

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DRAWING NUMBER
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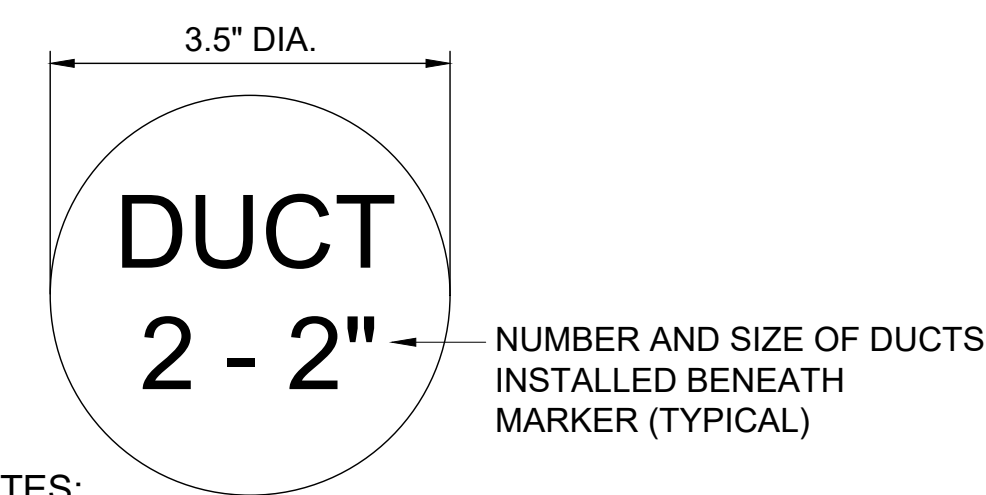
SHEET NUMBER
50

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BORING NOTES:

- POLYETHYLENE CONDUITS SHALL EXTEND 15'-0" BEYOND EACH PAVEMENT EDGE.
- INSTALL PE TO PVC FITTING AND EXTEND PVC RACEWAY SYSTEM TO MANHOLES, HAND HOLES, OR JUNCTION CANS AS INDICATED ON THE PLANS.
- INSTALL DUCT MARKERS AT EACH END FOR DUCT BORES AND SAW CUT LOCATIONS.
- INSTALL 3/4" x 10'-0" COPPERCLAD GROUND RODS AT EACH END OF ELECTRICAL DUCT AND BOND COUNTERPOISES USING EXOTHERMIC WELDS.
- BORING LOCATION SHALL BE MINIMUM 10'-0" BEYOND RUNWAY SAFETY AREA BOUNDARY UNLESS OTHERWISE NOTED. SEE PLANS FOR LOCATIONS AND COORDINATE WORK WITH ENGINEER PRIOR TO START.
- ADD CONCRETE AND BRONZE DUCT MARKERS AT BOTH ENDS OF BORE.
- SIMILAR FOR OTHER BORE SIZES.

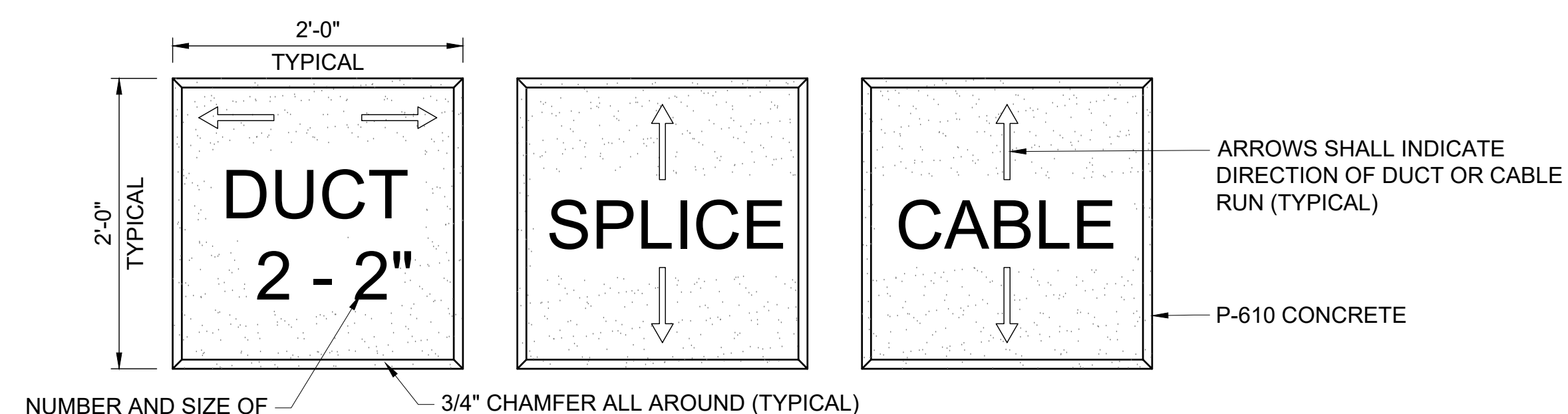


MARKER NOTES:

- PROVIDE 3.5" DIAMETER FLAT BRONZE MARKER, LETTERS 1/2" HIGH, RECESS MOUNTED FLUSH WITH TOP OF PAVEMENT, EPOXY IN PLACE.
- INSTALL MARKER CENTERED 6" FROM PAVEMENT EDGE.
- COST OF BRONZE MARKERS IS INCIDENTAL TO THE ASSOCIATED ITEMS OF DUCT OR CABLE.

1 DIRECTIONAL BORING CONDUITS
SCALE: NONE

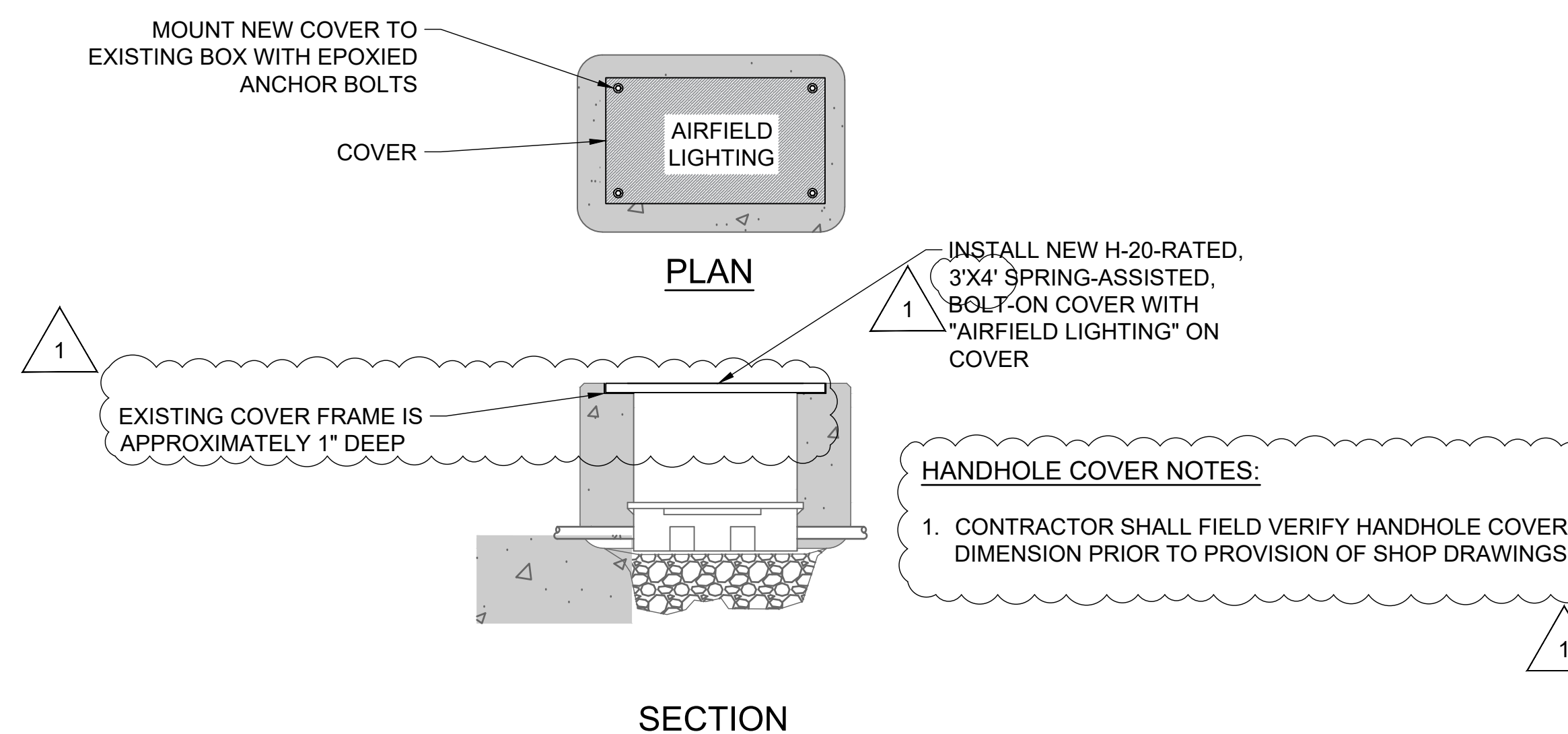
2 BRONZE PAVEMENT EDGE DUCT MARKER
SCALE: NONE



MARKER NOTES:

- LETTERS SHALL BE 4" HIGH, 3" WIDE, STROKE 1/2" WIDE, IMPRESSED 1/4" DEEP. LETTERS SHALL BE STENCILED.
- MARKERS SHALL BE 4" THICK MINIMUM AND PROJECT 1" AFG MAXIMUM.
- MARKERS SHALL BE PAINTED AVIATION ORANGE.
- COST OF CONCRETE MARKERS IS INCIDENTAL TO THE ASSOCIATED ITEMS OF DUCT OR CABLE.
- CABLE AND SPLICE MARKERS SHALL IDENTIFY THE CIRCUITS WHICH THE CABLES BELONG TO.
- EACH DUCT OF CONDUIT RUN SHALL BE MARKED APPROXIMATELY EVERY 200 FEET ALONG THE DUCT OR CONDUIT RUN, WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF DUCT OR CONDUIT RUN.
- EMPLOY THE FOLLOWING METHODS WHERE ADDITIONAL SPACE TO FIT THE LEGEND IS REQUIRED:
 - REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE.
 - INCREASE THE MARKER SIZE TO 30"X30" MAX.
 - PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE.

3 CONCRETE SLAB MARKERS
SCALE: NONE



4 HANDHOLE COVER DETAILS
SCALE: NONE