



**JEFFERSON COUNTY, TEXAS
PURCHASING DEPARTMENT**
1149 Pearl Street -First Floor
Beaumont, Texas 77701
409-835-8593 phone

ADDENDUM TO RFQ

RFQ Number: RFQ 19-024/JW

RFQ Title: On Call'Professional Airport Plan ning Services, Airport Master Plan Update, and Environmental Analysis for the Jack Brooks Regional Airport

RFQ Due: **11:00 am CT, Tuesday, April 30, 2019**

Addendum No.: 1

Issued (Date): April 24, 2019

TO BIDDER: This Addendum is an integral part of the RFQ package under consideration by you as a Bidder in connection with the subject matter herein identified. Jefferson County deems all sealed proposals to have been proffered in recognition and consideration of the entire RFQ package – **including all addenda**. For purposes of clarification, **receipt of this present Addendum by a Bidder should be evidenced by returning it (signed) as part of the Bidder's sealed proposal**. If the Proposal has already been received by the Jefferson County Purchasing Department, Bidder should return this addendum in a separate sealed envelope, clearly marked with the RFQ Title, RFQ Number, and Opening Date and Time, as stated above.

Reason for Issuance of this addendum:

- **Answers to Questions Received from Potential Respondents**
- **Additional Information: DBE Goals, Master Plan Initial Needs Determination, Existing Airport Layout Drawing, Property Map, Passenger/Operations/Air Cargo Data, Wildlife Hazard Management Plan**

The information included herein is hereby incorporated into the documents of this present Bid matter and supersedes any conflicting documents or portion thereof previously issued.

Receipt of this Addendum is hereby acknowledged by the undersigned Bidder:

ATTEST:

Authorized Signature (Bidder)

Witness

Title of Person Signing Above

Witness

Typed Name of Business or Individual

Approved by _____ Date: _____

Address

(RFQ 19-024/JW) Addendum No. 1 - Questions & Answers

Question: Do the required forms in the RFQ specifications (Pages 17-21: Vendor References, Signature Page, House Bill 89 Verification, Senate Bill 252 Certification, Respondents Certification) count towards the 30-page response limit noted in Section 5: Submittal Content?

Answer: No.

Question: Section 5: Submittal Content of the RFQ specifications states that the submittal shall include an acknowledgement of compliance with the applicable DBE and civil rights policies. Is there a DBE goal or DBE Certification Form?

Answer: Yes, there is a DBE goal. Please see Page 3 of this Addendum.

No, there is not a DBE Certification Form

SECTION 26.45: OVERALL GOAL CALCULATION

Name of Recipient: Jack Brooks Regional Airport

Goal Period: FY-2019 – 2021 (October 1, 2018 through September 30, 2021)

The Jack Brooks Regional Airport has calculated year-by-year Disadvantaged Business Enterprise (DBE) goals for 2019, 2020, and 2021. The goal calculations are attached to this overall goal summary. Each individual year's anticipated DOT-assisted contract amounts and DBE goals are represented below and are included in the calculation documentation for each individual year in the next three attachments. The overall DBE goal for the next three year period is an average of these goals and is set at 2.00% percent (median goal).

FY 2019 Anticipated DOT-assisted contract amount: \$300,000

DBE Goal: 2.00%

Design/Administration/Support = \$300,000 (100%)

Construction = \$0

Total dollar amount to be expended on DBE's: \$6,000

Design/Administration/Support = \$6,000 (2.00%)

Construction = \$0 (0%)

Describe the Number and Type of Projects for DBE goal calculations:

FY19: Design Taxiway A (RW 12 to TW B)

FY19: Design Lighting System Upgrade

FY19: Design Passenger Boarding Bridge

FY19: Design Vault Repackage

FY19: Planning Master Plan for Primary Shift

FY 2020 Anticipated DOT-assisted contract amount: \$1,000,000

DBE Goal: 4.81%

Design/Administration/Support = \$0 (0%)

Construction = \$1,000,000 (100%)

Total dollar amount to be expended on DBE's: \$48,100

Design/Administration/Support = \$0 (0%)

Construction = \$48,100 (100%)

Describe the Number and Type of Projects for DBE goal calculations:

FY20: Construction Taxiway A (RW 12 to TW B)

FY20: Construction Lighting System



Jack Brooks Regional Airport (BPT) Master Plan Update Initial Needs Determination January 2019

Executive Summary

Jack Brooks Regional Airport (BPT) is part of the Federal Aviation Administration (FAA) National Plan of Integrated Airport Systems (NPIAS). BPT is categorized as a Non-hub commercial service airport with annual enplanements that range between 23,000 and 26,000 passengers per year.

BPT is an essential mode of transportation in Jefferson County, Texas, the surrounding counties, and the Southeastern portions of the Texas Gulf Coast. Jefferson County is part of the Beaumont – Port Arthur, Texas metropolitan area. According to the 2015 US Census the estimated population of Jefferson County is 254,308.

BPT serves an important role in the NPIAS since it is the only commercial service airport in Southeast Texas. BPT is owned and operated by Jefferson County, Texas. BPT is located southwest of the city of Nederland, Texas in an unincorporated area of Jefferson County, Texas. BPT is currently served by Envoy Air Inc. (formerly American Eagle) on behalf of American Airlines.

Jefferson County will need to conduct a comprehensive study of the airport to evaluate and prepare for the short, medium, and long-term developmental needs of the airfield as well as future aviation demand. The current FAA approved Airport Master Plan was completed in 2007. A Master Plan Update with all applicable sections of FAA AC 150/5070-6B Airport Master Plans is needed. A Master Plan Update must also place emphasis on ensuring the airport is enhanced in terms of all applicable sections of FAA AC 150/5300-13A Airport Design.

This initial needs determination document lists the various factors that justify the need for an Airport Master Plan Update. This document will also assist Jefferson County with developing the scope for the Airport Master Plan Update as well as assist the County with the preparation of the Request for Qualifications for a potential consulting and planning firm to undertake the study.

Needs Determination and reasons for an Airport Master Plan Update

1.0 Potential changes in the air carrier aircraft fleet within the next 5 – 7 years

On November 13, 2015, Envoy began accepting deliveries on a new Embraer E175 (76 passenger) aircraft with 90 configuration options. Envoy took delivery of an additional two Embraer E175 in October 2016. In May 2018 they announced they would take



delivery of 15 additional E175s and in November 2018 they announced they would take and additional 15 E175, giving them a total of 84 E175s by mid-2020.

Jefferson County would benefit from these larger aircraft and total enplanements could increase over the next 5 – 7 years. Therefore, an Airport Master Plan Update should research and evaluate airfield changes that would accommodate a potential change of aircraft equipment.

Some of the local aviation tenants may also purchase new jet aircraft (an example aircraft type is the Cessna Citation XL) and potentially increase their operations within the next 5 – 7 years.

2.0 Jefferson County Diverse Economy and Aviation Opportunities

BPT strategic goal is to continue to be an essential part of the National transportation system because of the direct and indirect connectivity to:

1. Interstate Highway 10
2. US Highways 90 and 69-96-287
3. State Highways 73, 87, and 105 and 3 farm-to-market roads
4. Rail and motor freight carriers
5. Deep-water ports located in:
 - a. Beaumont, Texas
 - b. Port Arthur, Texas
 - c. Orange, Texas
 - d. Sabine Pass, Texas

Note: The Sabine Neches Waterway is the 3rd largest port in the United States by tonnage and offers deep-water access to ocean-going vessels.

BPT is uniquely located to efficiently serve the residents and visitors of Jefferson County, the surrounding counties, and the vibrant industrial and petrochemical companies in the area. Corporations in the county are involved in the refining of petroleum, the production of bio-fuels and other chemicals, steel fabrication, agricultural products, and health care services. Some refineries have recently announced major plant expansions within the next 1 - 5 years. Notable corporations in the Jefferson County area include:

- Air Products
- Exxon Mobil
- Golden Pass LNG Terminal
- Lucite International, LLC
- OCI Beaumont, LLC
- Phillips 66 Beaumont Terminal
- The Vitol Group
- Sempra Energy



3.0 Airport Strategic Vision

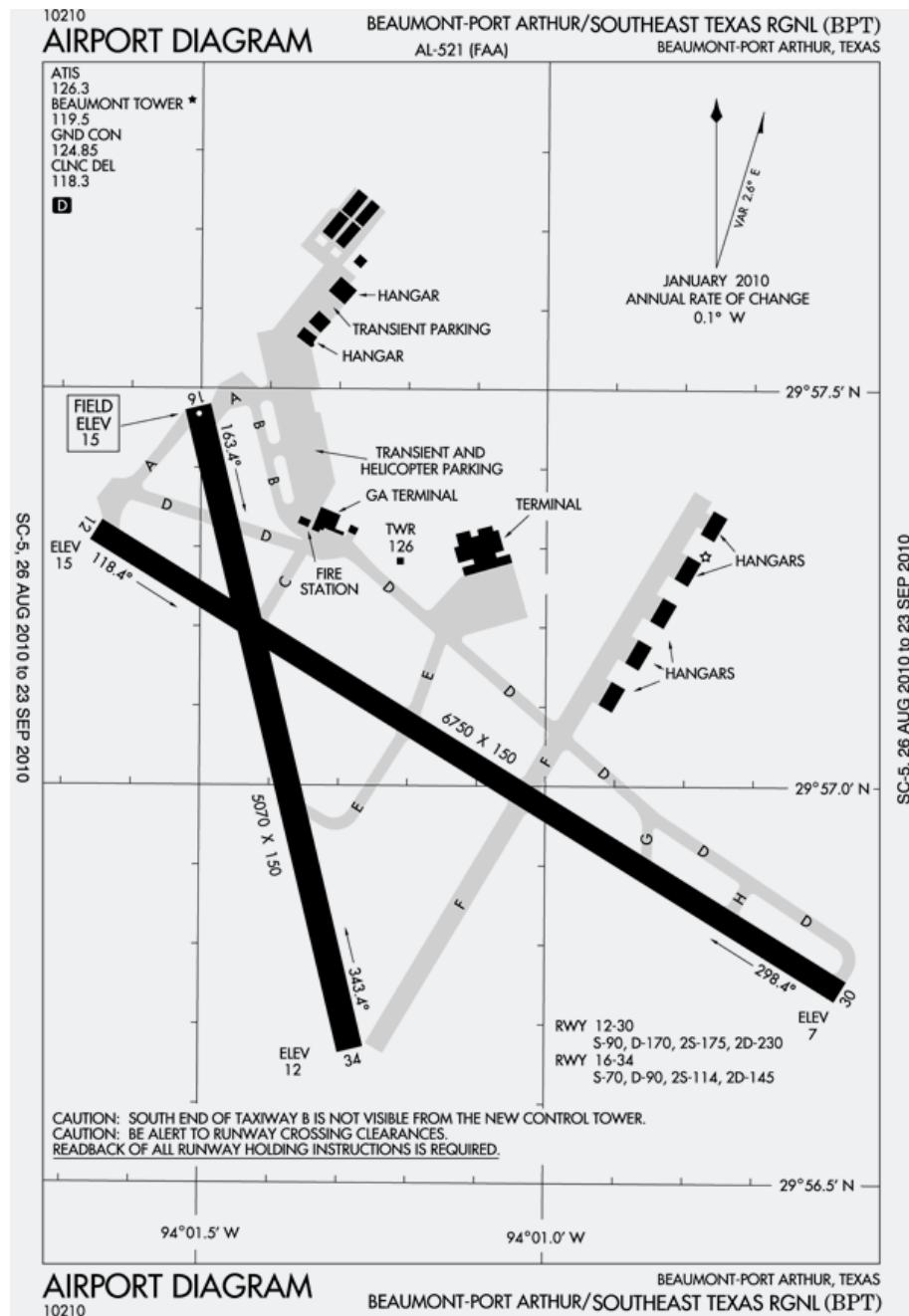
Jefferson County's strategic vision for the airport is to increase the number of annual enplaned passengers and increase aircraft operations while continuing to improve the safety and efficiency of the airfield. The vision also includes continuing the self-sufficiency of the airport by creating more opportunities to increase both airside and landside revenue streams.

Jefferson County can achieve this vision by changing the airfield geometry in a manner that enhances airport safety and efficiency in an environmentally responsible manner. Jefferson County also needs to ensure the airfield meets all applicable FAA standards to the extent practical. An Airport Master Plan Update is necessary to help BPT do the necessary strategic planning to achieve the following objectives (as a minimum):

- 3.1 Standardizing the airfield geometry will ensure BPT is prepared for FAA NextGEN capabilities and technologies where applicable. This includes improving the airport surface by eliminating the intersection of the runways at BPT.
- 3.2 BPT can potentially increase safety both on and off the airfield by eliminating the overlapping runways and by shifting the RPZs onto airport property.
- 3.3 BPT can potentially improve the airport geometry so that runway and taxiway components fully match current FAA standards and FAA advisory circular AC 150/5300-13A – Airport Design. This includes addressing the long standing problem of intersecting runways and fixing the taxiway Foxtrot connection to runway 34.
- 3.4 BPT can potentially create a configuration that would be less confusing to itinerant pilots and reduce the possibility of wrong runway departure by shifting the thresholds of runways 16/34 and 12/30. BPT currently has sufficient land within the airport boundary to shift the crossing runways beyond the current intersection point without overlapping newly created runway safety areas (RSA).
- 3.5 BPT can potentially reduce the possibility of runway incursions by shifting the thresholds of runways 16/34 and 12/30. This shift would eliminate an ATCT "blind spot" on a portion of taxiway Bravo adjacent to the GA ramp. This "blind spot" was identified in a recent BPT Runway Safety Action Team (RSAT) meeting. This shift will also give BPT an opportunity to create new and understandable pavement signs and markings.
- 3.6 BPT can protect future approaches as well as prevent incompatible land uses if a new Airport Layout Drawing (ALD) is developed as part of a Master Plan Update.
- 3.7 BPT can potentially develop a plan that would address the current age and condition of the existing airfield pavements.



3.8 BPT could potentially shift the noise contours away from residential areas west and northwest of the airfield.



4.0 Airfield development opportunities due to existing compatible land uses



BPT is located on approximately 1180 acres, and is currently surrounded by compatible land uses including residential, light commercial, and light industrial real estate and properties. BPT periodically receives requests for routing of utilities to service residential areas as well as requests for new pipelines that serve the petrochemical industries near the airport.

BPT is responsible for protecting the airfield imaginary surfaces described in 14 CFR Part 77 — Safe, Efficient Use, and Preservation of Navigable Airspace. However, protecting the airspace any future airfield developmental improvements is not possible without a new Airport Layout Plan (ALP) and a new Airport Layout Drawing (ALD) depicting the needed improvements.

Updating the Airport Master Plan at this time will help Jefferson County make the appropriate decisions and determinations when various entities seek to use land adjacent to the airport. A BPT Airport Master Plan Update will also need to include the development of a new ALP.

5.0 Emergency planning, response, and partnerships

BPT is a participating member of the Southeast Airport Disaster Operations Group (SEADOG). BPT has partnered with other SEADOG airports along the Gulf Coast and the Atlantic Ocean to receive assistance as well as offer support to airports that are impacted by natural disasters such as hurricanes or floods. During a crisis, BPT can offer specific expertise and well as temporary storage or staging for relief operations. A comprehensive airport planning effort can potentially identify effective staging areas for emergency operations.

6.0 Summary and Needs Determination

The existing Airport Master Plan was completed in 2007. For this reason and for reasons stated in this document, it has been determined that an Airport Master Plan Update is needed at Jack Brooks Regional Airport.

Jefferson County, Texas should seek a qualified consulting firm or team to provide professional on-call airport planning services to develop a comprehensive **Airport Master Plan Update and related Environmental Analysis** for Jack Brooks Regional Airport (BPT). See Table 1 for an approximate RFQ solicitation award schedule and timeline.



Table 1
Proposed schedule for the RFQ solicitation and potential award of an
“On-Call” Airport Planning Consultant or Team

BPT Proposed RFQ Solicitation and Award Timeline		
Task	Date*	Remarks
1. RFQ Solicitation release date	03/25/19	
2. Non-mandatory pre-submittal meeting	04/16/19	4+ weeks at 1:30 p.m.
3. Last Day for Questions	04/26/19	
4. Submittal submission deadline	04/30/19	By 4:00 p.m.
5. Technical review of submittals	05/10/19	Round 1
6. Technical review of submittals	05/17/19***	Round 2 (if necessary)
7. All interviews scheduled	05/31/19**	
8. Award Notification (approximate)	06/28/19	
9. Execution of Agreement (approximate)	08/12/19	45 days (estimated)

(*) = actual dates may vary per County approval process

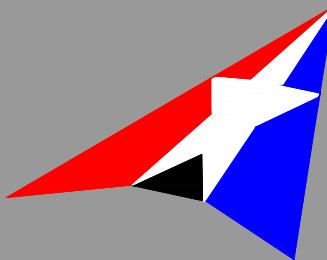
(**) = interview scheduling may dictate actual date(s)

(***) = if necessary



Airport Layout Plan

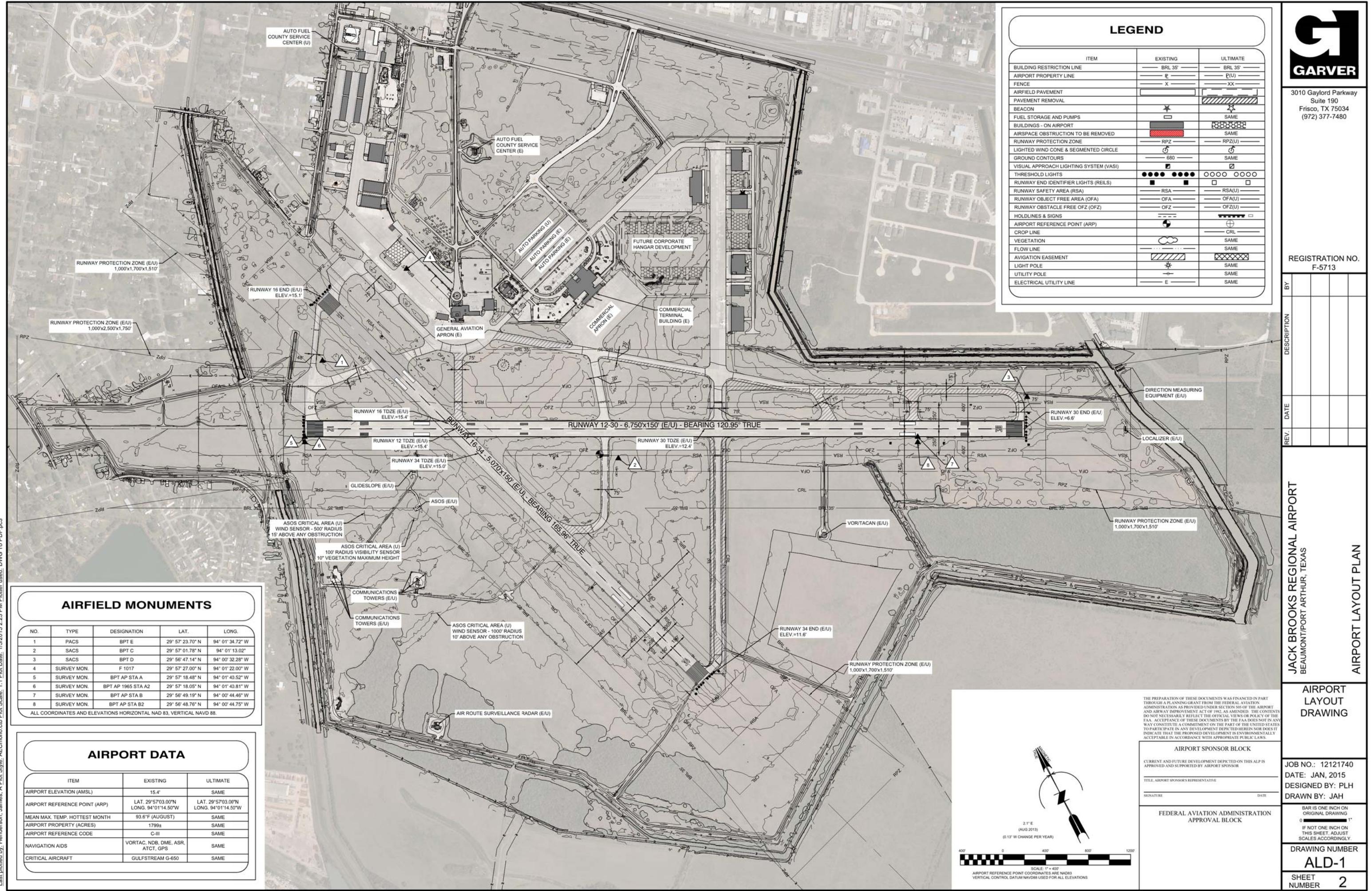
Jack Brooks Regional Airport
Beaumont / Port Arthur, Texas

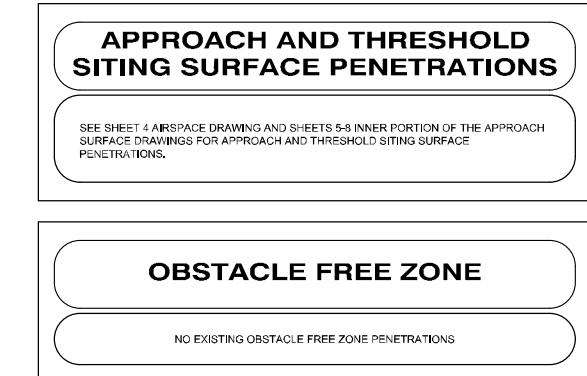
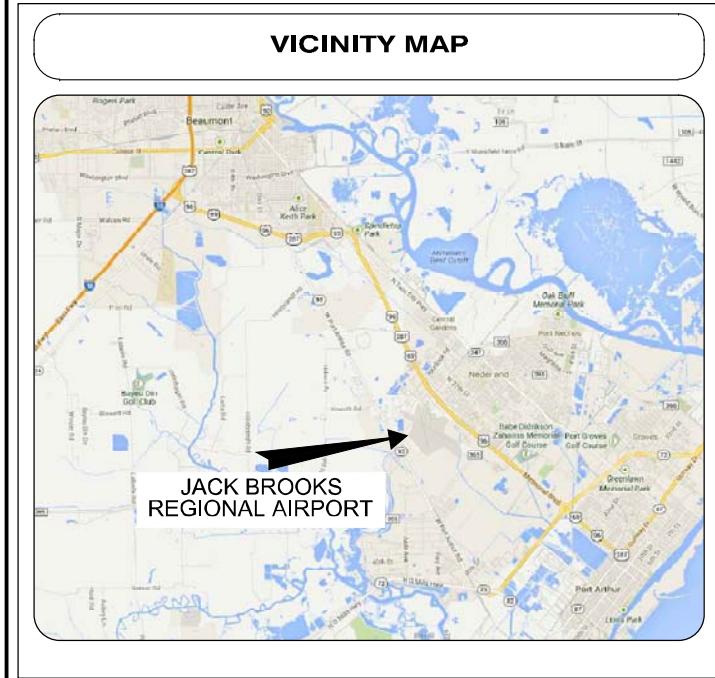


January, 2015



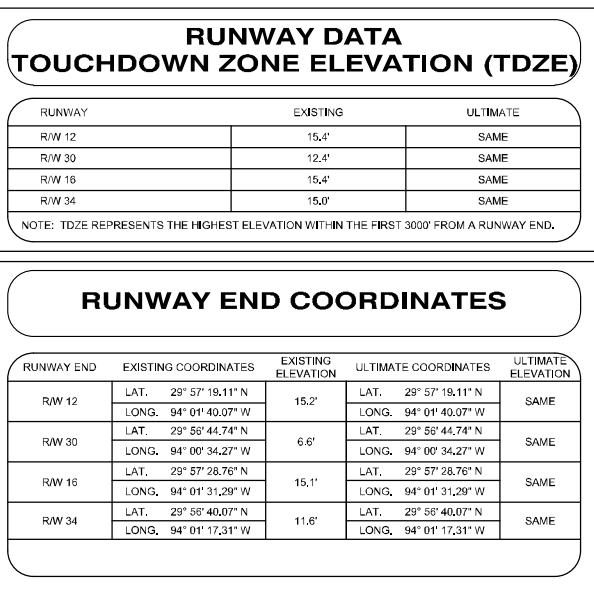
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Sheet Number	DRAWING NUMBER	Sheet Title
COVER		
1	COV	Cover Sheet
AIRPORT LAYOUT		
2	ALD-1	Airport Layout Drawing
3	ALD-2	Airport Layout Drawing Tables
AIRSPACE		
4	ASD	Airspace Drawing
INNER PORTION OF THE APPROACH SURFACE		
5	IPASD-12	Inner Portion of the Approach Surface - Runway 12
6	IPASD-30	Inner Portion of the Approach Surface - Runway 30
7	IPASD-16	Inner Portion of the Approach Surface - Runway 16
8	IPASD-34	Inner Portion of the Approach Surface - Runway 34
9	IAOT	Inner Approach Obstruction Tables
TERMINAL AREA		
10	TAKM	Terminal Area Key Map
11	TAD-1	Terminal Area Drawing I
12	TAD-2	Terminal Area Drawing II
13	TAD-3	Terminal Area Drawing III
LAND USE		
14	LUD	Land Use Drawing
AIRPORT PROPERTY		
15	APKM	Airport Property Key Map
16	APM-1	Airport Property Map I
17	APM-2	Airport Property Map II
18	APM-3	Airport Property Map III
19	APM-4	Airport Property Map IV
20	APM-5	Airport Property Map V
21	APM-6	Airport Property Map VI
22	APM-7	Airport Property Map VII
23	APT	Airport Property Tables





RUNWAY DATA

ITEM	RUNWAY 12/30		RUNWAY 16/34	
	EXISTING	ULTIMATE	EXISTING	ULTIMATE
APPROACH VISIBILITY MINIMUMS	PIR / D	SAME	D / D	SAME
PART 77 APPROACH SURFACES	50:1 / 34:1	SAME	34:1 / 34:1	SAME
FAR PART 77 CATEGORY	PIR / D	SAME	D / D	SAME
RUNWAY WIDTH AND LENGTH	6,750' X 150'	SAME	5,070' X 150'	SAME
PAVEMENT TYPE	CONCRETE - GRVD	SAME	CONCRETE - GRVD / ASPH	CONCRETE - GRVD
PAVEMENT STRENGTH	90,000 LBS SWG	SAME	70,000 LBS SWG	SAME
RUNWAY LIGHTING	HIRL	SAME	HIRL	SAME
RUNWAY MARKING	PIR	SAME	NPI	SAME
EFFECTIVE RUNWAY GRADIENT %	0.1%	SAME	0.1%	SAME
MAXIMUM GRADE WITHIN RUNWAY LENGTH	0.1%	SAME	0.1%	SAME
RUNWAY LINE-OF-SIGHT	CRITERIA MET	SAME	CRITERIA MET	SAME
VISUAL APPROACH AIDS	MALSR / VASI-4L	SAME	VASI-4L	SAME
INSTRUMENT APPROACH AIDS	ILS, RNAV / RNAV	SAME	RNAV	SAME
AIRPORT REFERENCE CODE (ARC)	C-III	SAME	B-III	SAME
RUNWAY SAFETY AREA	500' X 8,750'	SAME	300' X 8,270'	SAME
RUNWAY OBJECT FREE AREA	800' X 8,750'	SAME	800' X 8,270'	SAME
OBSTACLE FREE ZONE	800' X 7,150'	SAME	400' X 5,470'	SAME
TAXIWAY MARKINGS	STANDARD/CENTERLINE	SAME	STANDARD/CENTERLINE	SAME
TAXIWAY LIGHTING	MTL	SAME	MTL	SAME
TAXIWAY WIDTH	75'	SAME	75'	SAME
% WIND COVERAGE	99.93%	SAME	99.93%	SAME
AERONAUTICAL SURVEY TYPE	18B	-	18B	-

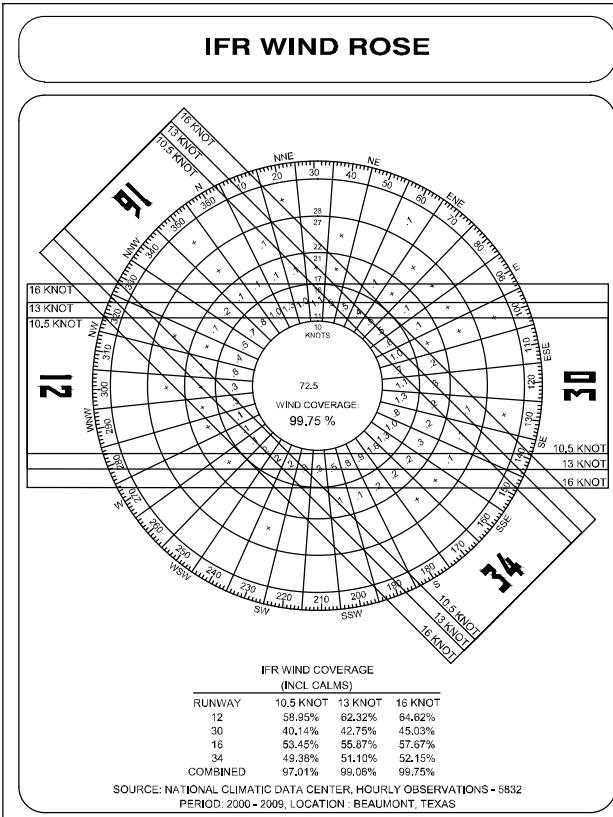
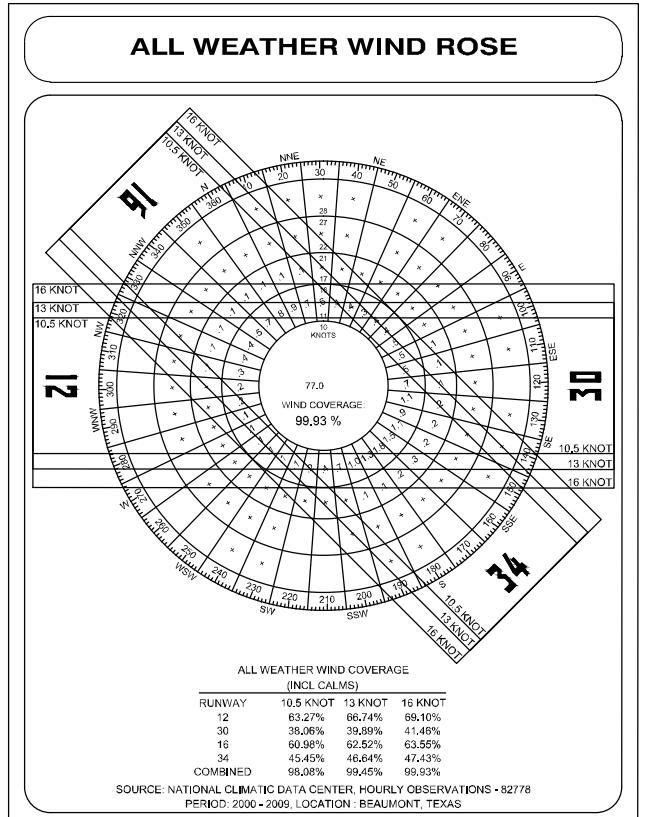


RUNWAY DATA DECLARED DISTANCES

	RUNWAY 12/30 EXISTING	RUNWAY 12/30 ULTIMATE	RUNWAY 16/34 EXISTING	RUNWAY 16/34 ULTIMATE
TAKEOFF RUN AVAILABLE (TORA)	6,750' / 6,750'	SAME	5,070' / 5,070'	SAME
TAKEOFF DISTANCE AVAILABLE (TODA)	6,750' / 6,750'	SAME	5,070' / 5,070'	SAME
ACCELERATE-STOP DISTANCE AVAILABLE (ASDA)	6,750' / 6,750'	SAME	5,070' / 5,070'	SAME
LANDING DISTANCE AVAILABLE (LDA)	6,750' / 6,750'	SAME	5,070' / 5,070'	SAME

NON-STANDARD CONDITIONS

ITEM	AIRPORT REFERENCE CODE	STANDARD		NON-STANDARD CONDITION		REMARKS
		EXISTING	ULTIMATE	EXISTING	ULTIMATE	
NO NON-STANDARD CONDITIONS EXIST						



G
GARVER
3010 Gaylord Parkway
Suite 190
Frisco, TX 75034
(972) 377-7480

REGISTRATION NO.
F-5713

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS
AIRPORT LAYOUT PLAN

THE PREPARATION OF THESE DOCUMENTS WAS FINANCED IN PART
THROUGH A PLANNING GRANT FROM THE FEDERAL AVIATION
ADMINISTRATION (FAA) AND IN PART BY THE STATE OF TEXAS
AND AIRPORT IMPROVEMENT ACT OF 1982, AS AMENDED. THE CONTENTS
DO NOT NECESSARILY REFLECT THE OFFICIAL VIEWS OR POLICY OF THE
FAA, STATE OF TEXAS, OR AIRPORT AUTHORITY. THE INFORMATION
HEREIN CONSTITUTES A COMMITMENT ON THE PART OF THE UNITED STATES
TO PARTICIPATE IN ANY DEVELOPMENT (UP TO 10%) WHICH, NOR DOES IT
INDICATE THAT THE PROPOSED DEVELOPMENT IS ENVIRONMENTALLY
ACCEPTABLE IN ACCORDANCE WITH APPROPRIATE FEDERAL LAWS.

APPROVAL BLOCK

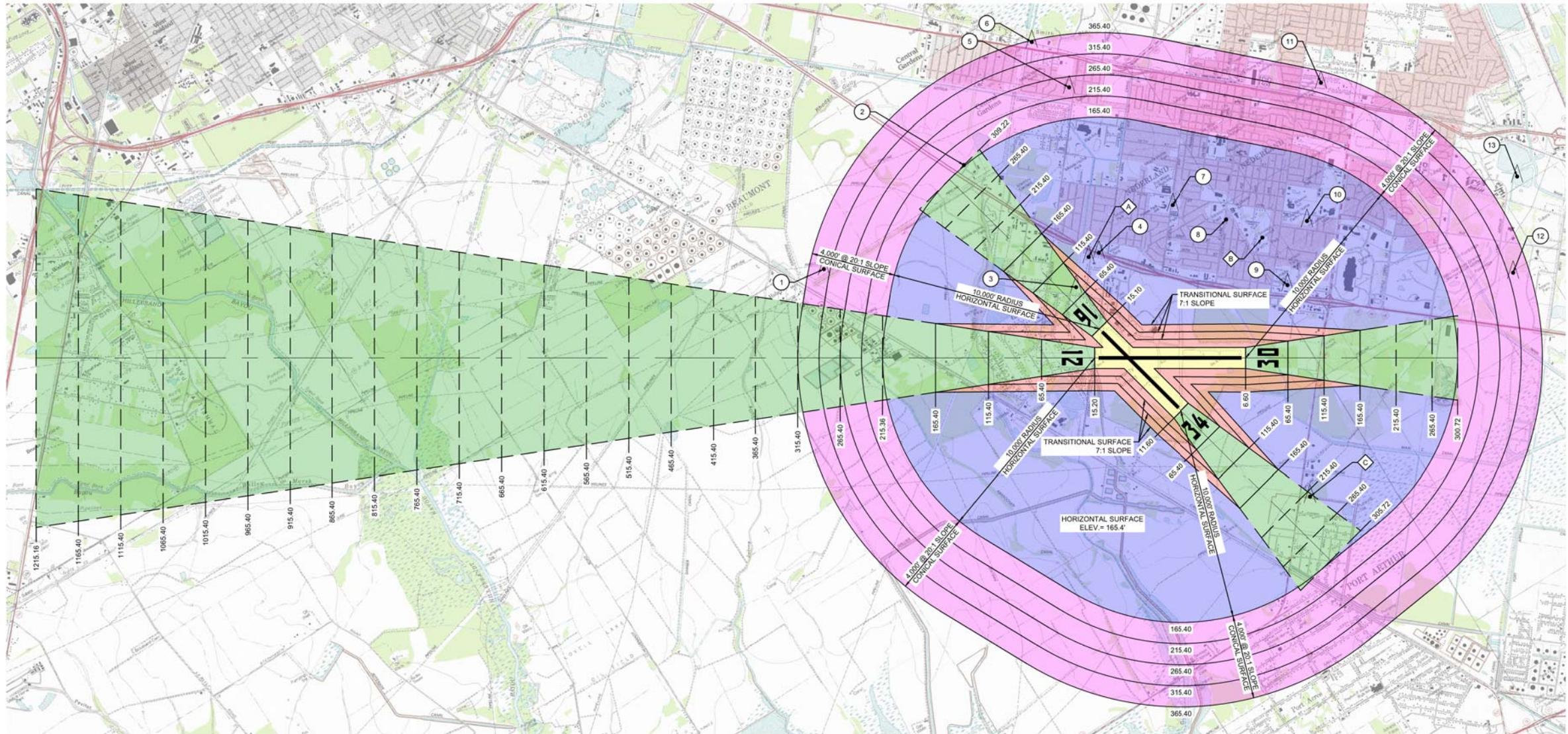
CURRENT AND FUTURE DEVELOPMENT DEPICTED ON THIS AUP IS
APPROVED AND SUSPENDED BY AIRPORT SPONSOR

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

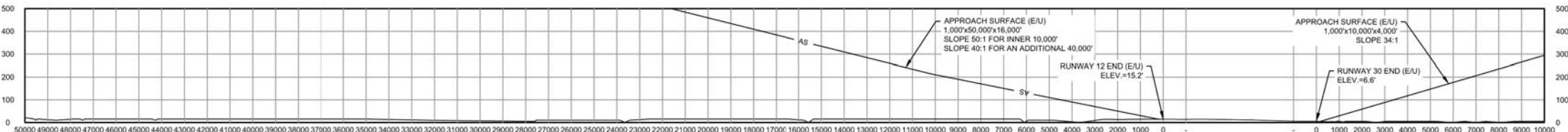
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ORIGINAL DRAWING
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IF NOT ONE INCH ON
THIS SHEET, ADJUST
SCALES ACCORDINGLY.

DRAWING NUMBER
ALD-2

SHEET
NUMBER
3



RUNWAY 12-30 PROFILE

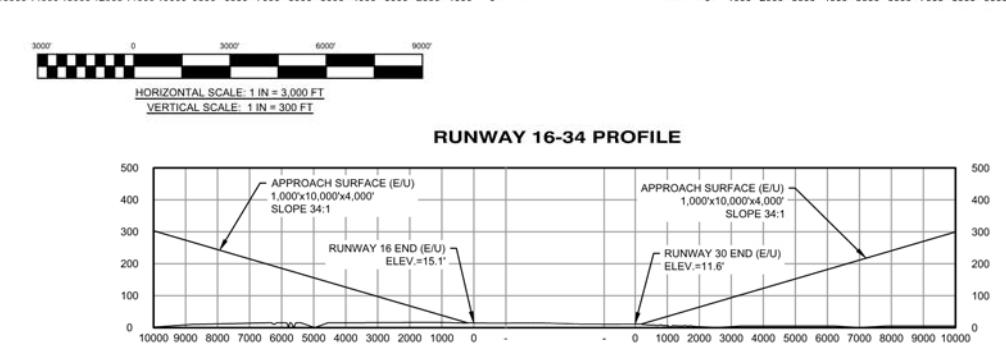


AIRSPACE CLEARANCE TABLE

DESCRIPTION	TOP ELEVATION	SURFACE	CLEARANCE IN FEET	LOCATION	FAA STUDY NUMBER
① TOWER	158' (141' AGL)	CONICAL	172.3'	29° 58' 16.00"N, 94° 03' 22.00"W	0069_HU005220E
② POLE	141' (126' AGL)	HORIZONTAL	24.4'	29° 59' 00.85"N, 94° 01' 48.80"W	2007ASW008610E
③ TOWER	75' (60' AGL)	APPROACH	2.9'	29° 57' 44.00"N, 94° 01' 31.00"W	0066_HU007080E
④ TOWER	129' (114' AGL)	HORIZONTAL	36.4'	29° 57' 53.00"N, 94° 01' 11.00"W	1973ASW010930E
⑤ TOWER	120' (100' AGL)	CONICAL	120.1'	29° 59' 09.00"N, 94° 00' 39.00"W	-
⑥ TOWER	208' (189' AGL)	CONICAL	153.7'	29° 59' 14.76"N, 94° 00' 43.63"W	2009ASW033000E
⑦ TOWER	161' (145' AGL)	HORIZONTAL	4.4'	29° 58' 02.10"N, 94° 00' 23.80"W	2007ASW097560E
⑧ RIG	161' (151' AGL)	HORIZONTAL	4.4'	29° 57' 28.00"N, 94° 00' 03.00"W	-
⑨ TOWER	131' (125' AGL)	HORIZONTAL	34.4'	29° 56' 51.31"N, 93° 59' 52.92"W	2011ASW034530E
⑩ TOWER	165' (155' AGL)	HORIZONTAL	0.4'	29° 57' 02.88"N, 93° 59' 26.21"W	2010ASW051760E
⑪ TOWER	172' (159' AGL)	CONICAL	167.4'	29° 58' 02.76"N, 93° 58' 41.62"W	2011ASW014650E
⑫ T-L TOWER	107' (105' AGL)	CONICAL	220.8'	29° 55' 43.16"N, 93° 58' 05.74"W	2009ASW012220E
⑬ T-L TOWER	110' (105' AGL)	CLEAR OF ALL	-	29° 56' 13.65"N, 93° 57' 37.27"W	2009ASW012130E

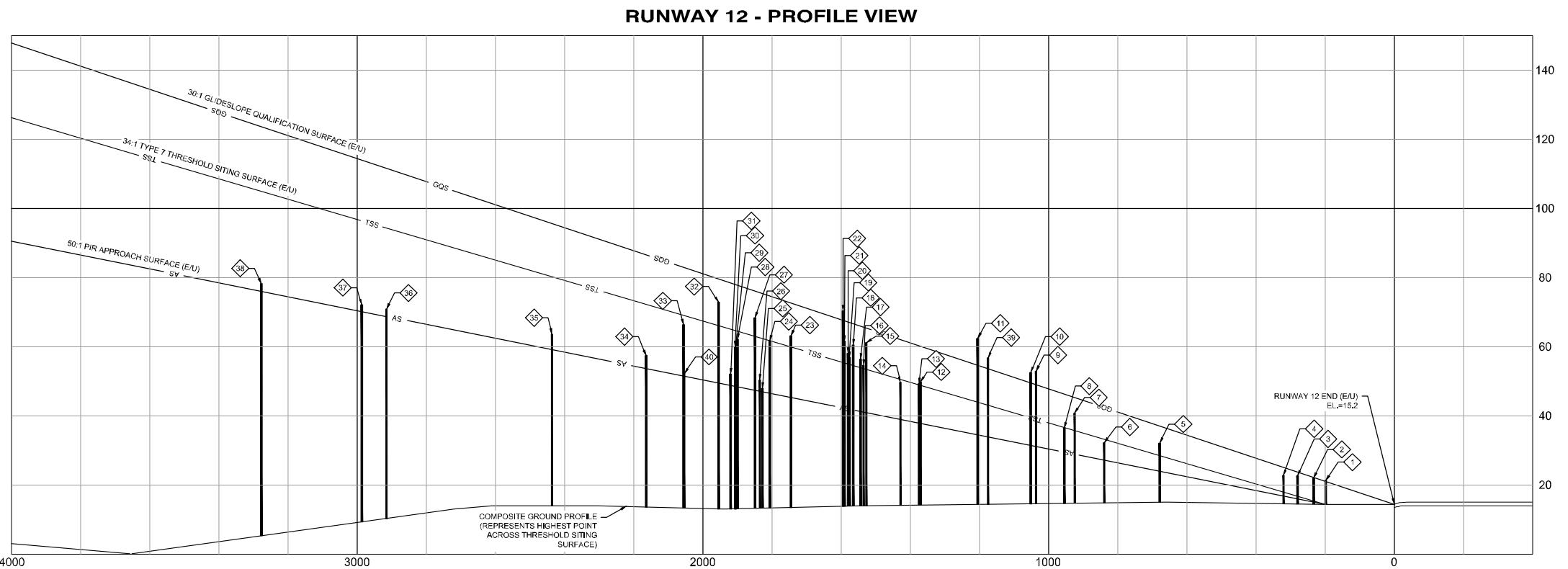
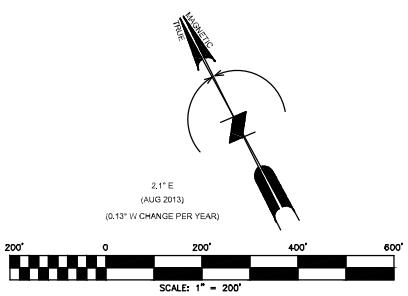
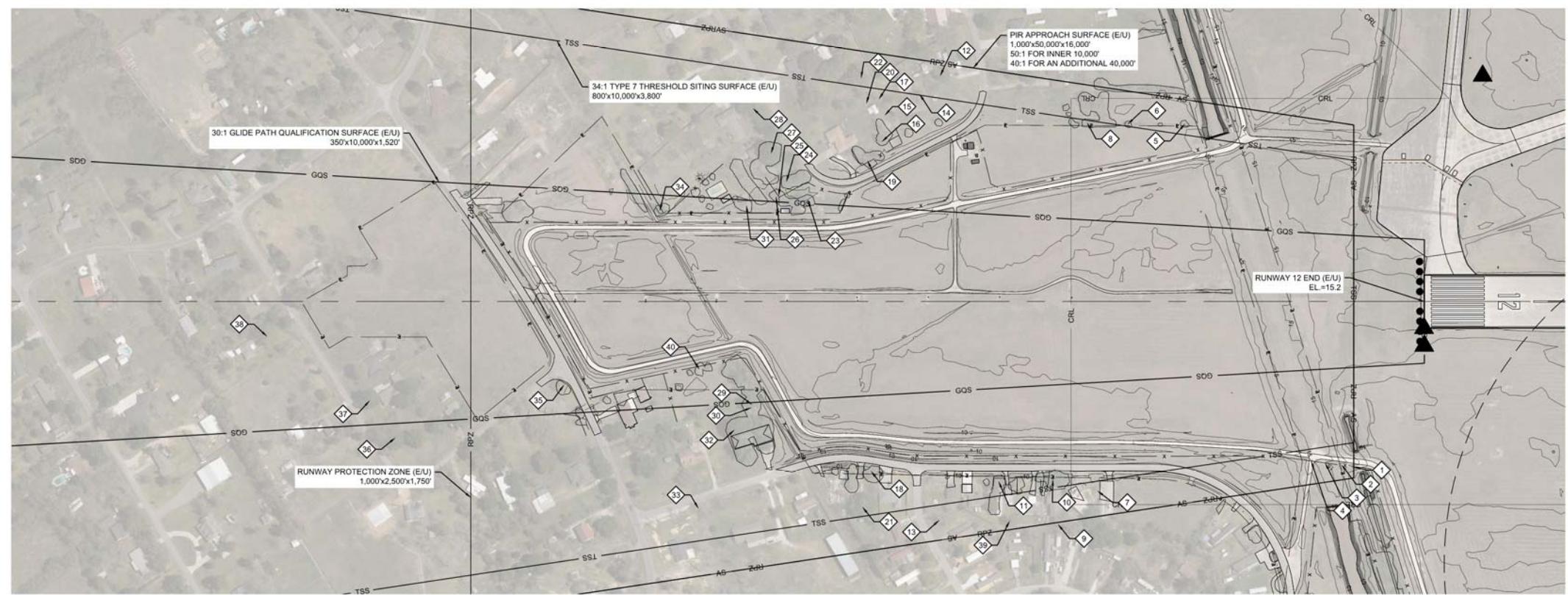
HORIZONTAL SCALE: 1 IN = 3,000 FT
VERTICAL SCALE: 1 IN = 300 FT

RUNWAY 16-34 PROFILE



AIRSPACE OBSTRUCTION/DISPOSITION TABLE

DESCRIPTION	TOP ELEVATION	SURFACE	PENETRATED	LOCATION	FAA STUDY NUMBER	DISPOSITION
④ LIGHTED TOWER	170' (152' AGL)	HORIZONTAL	4.6'	29° 58' 01.40"N, 94° 01' 16.90"W	2009ASW007100E	NONE
⑤ LIGHTED TANK	170' (160' AGL)	HORIZONTAL	4.6'	29° 57' 02.88"N, 93° 59' 26.21"W	2010ASW051760E	NONE
⑥ TOWER	188' (185' AGL)	HORIZONTAL	22.6'	29° 54' 13.00"N, 94° 00' 41.00"W	2003ASW057370E	NONE



LEGEND

ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 15'	BRL 35'
AIRPORT PROPERTY LINE	—	—
FENCE	X	XX
AIRFIELD PAVEMENT	—	—
PAVEMENT REMOVAL	—	—
BEACON	★	★
FUEL STORAGE AND PUMPS	—	SAME
BUILDINGS - ON AIRPORT	RPZ	RPZ(U)
RUNWAY PROTECTION ZONE	—	—
APPROACH SURFACE	AS	AS(U)
THRESHOLD SITING SURFACE	TSS	TSS(U)
GLIDEPATH QUALIFICATION SURFACE	GQS	GQS(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	—	SAME
GROUND CONTOURS	680	SAME
PRECISION APPROACH PATH INDICATOR (PAPI)	■■■■	■■■■
THRESHOLD LIGHTS	●●●●	○○○○
RUNWAY END IDENTIFIER LIGHTS (REILS)	—	—
HOLDLINES & SIGNS	—	—
AIRPORT REFERENCE POINT (ARP)	—	—
CROP LINE	—	—
VEGETATION	—	SAME
FLOW LINE	—	SAME
AVIGATION EASEMENT	—	—
LIGHT POLE	—	SAME
UTILITY POLE	—	SAME
ELECTRICAL UTILITY LINE	E	SAME

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS
AIRPORT LAYOUT PLAN

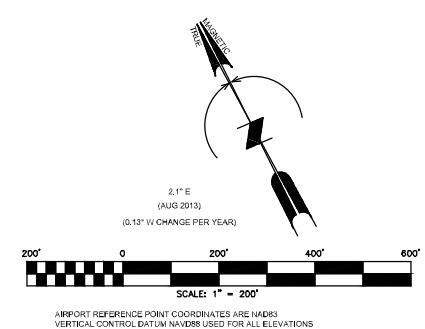
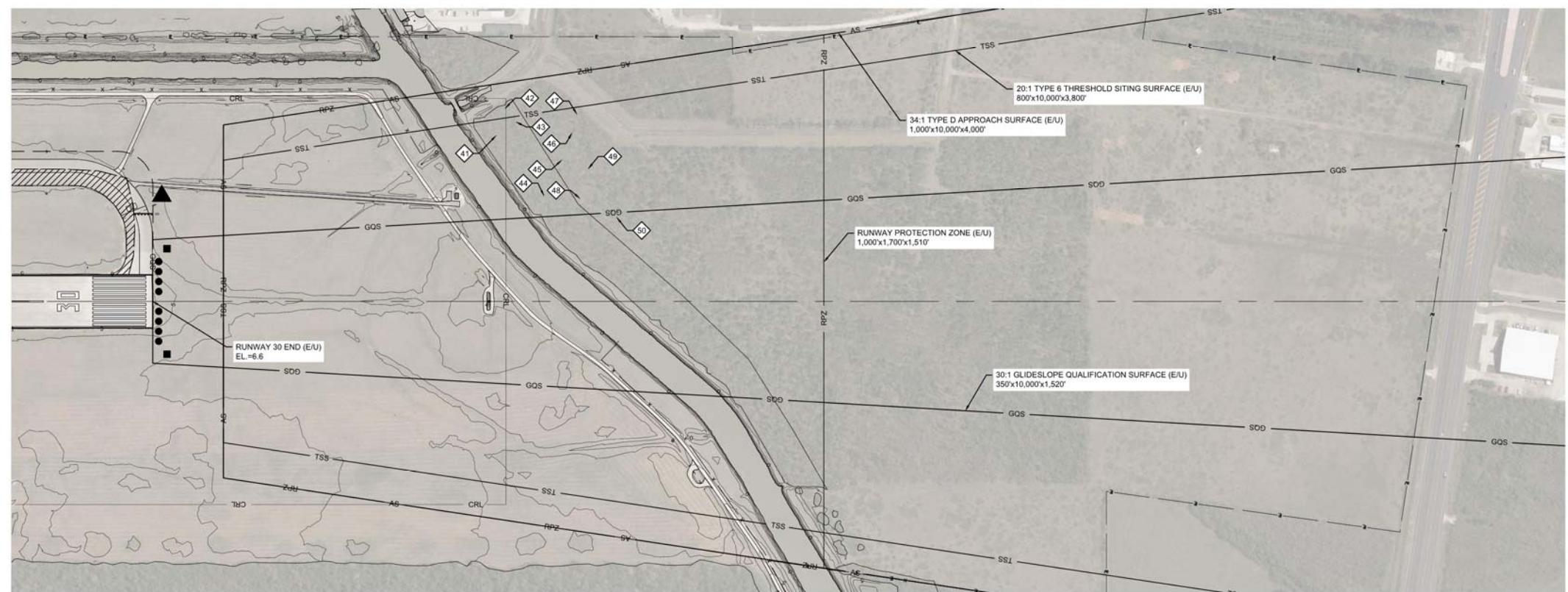
INNER PORTION
OF THE
APPROACH
SURFACE -
RUNWAY 12

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

BAR IS ONE INCH ON
ORIGINAL DRAWING
0 1"
IF NOT ONE INCH ON
THIS SHEET, ADJUST
SCALES ACCORDINGLY.

DRAWING NUMBER
IPASD-12

SHEET
5

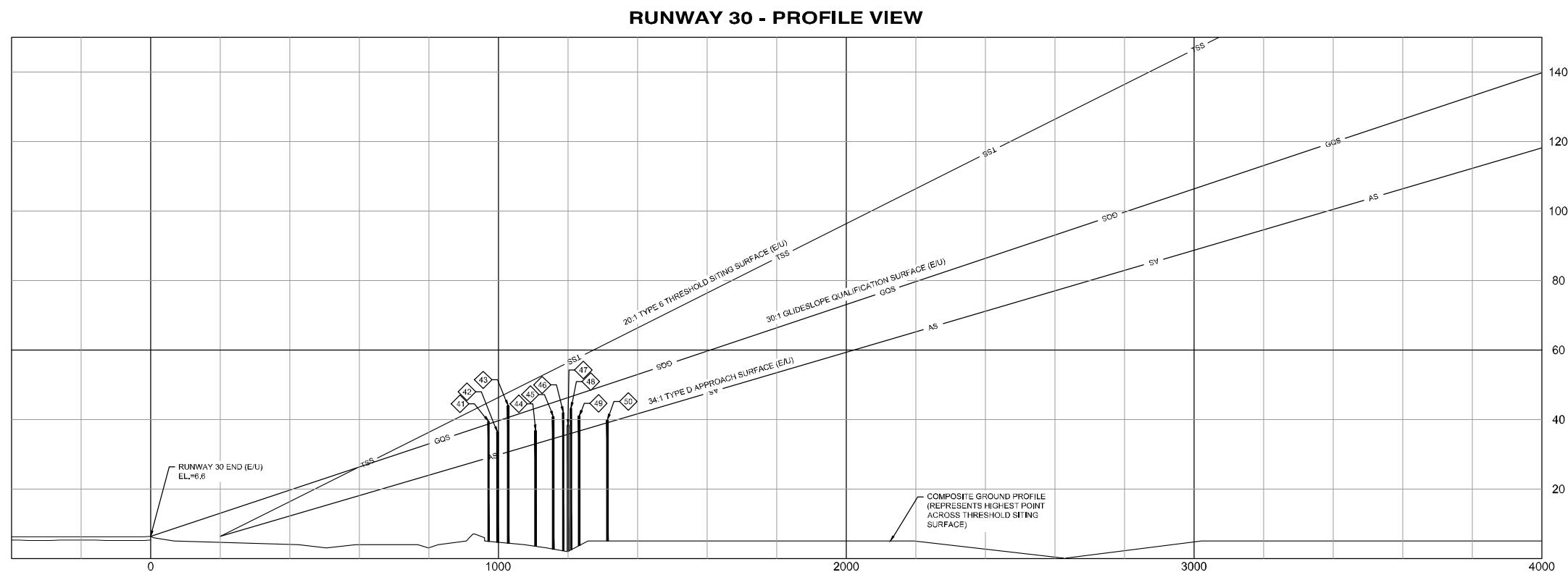


NOTES:

- SEE SHEET 9 - INNER APPROACH OBSTRUCTION TABLES FOR RUNWAY 34 INNER APPROACH OBSTRUCTION/DISPOSITION TABLE.
- AERIAL PHOTOGRAPH AND OBSTRUCTION SURVEY PROVIDED BY SAM, INC. JULY 2013.

REGISTRATION NO.
F-5713

REV.	DATE	DESCRIPTION	BY



LEGEND

ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 15'	BRL 35'
AIRPORT PROPERTY LINE	RPZ	RPZ(U)
FENCE	X	XX
AIRFIELD PAVEMENT		
PAVEMENT REMOVAL		
BEACON	★	★
FUEL STORAGE AND PUMPS		SAME
BUILDINGS - ON AIRPORT	RPZ	RPZ(U)
APPROACH SURFACE	AS	AS(U)
THRESHOLD SITING SURFACE	TSS	TSS(U)
GLIDESLOPE QUALIFICATION SURFACE	GOS	GOS(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	○	SAME
GROUND CONTOURS	680	SAME
PRECISION APPROACH PATH INDICATOR (PAPI)	■■■■	□□□□
THRESHOLD LIGHTS	●●●●	○○○○
RUNWAY END IDENTIFIER LIGHTS (REILS)	■	□
HOLDLINES & SIGNS	—	—
AIRPORT REFERENCE POINT (ARP)	●	⊕
CROP LINE	—	CRL
VEGETATION	○○○○	SAME
FLOW LINE	...	SAME
AVIGATION EASEMENT	■■■■	XXXXXX
LIGHT POLE	★	SAME
UTILITY POLE	—	SAME
ELECTRICAL UTILITY LINE	E	SAME

BAR IS ONE INCH ON ORIGINAL DRAWING
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JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS
AIRPORT LAYOUT PLAN

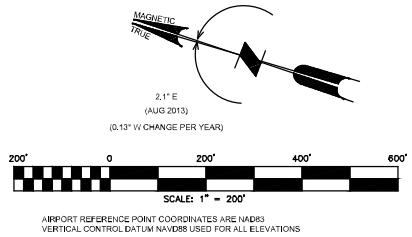
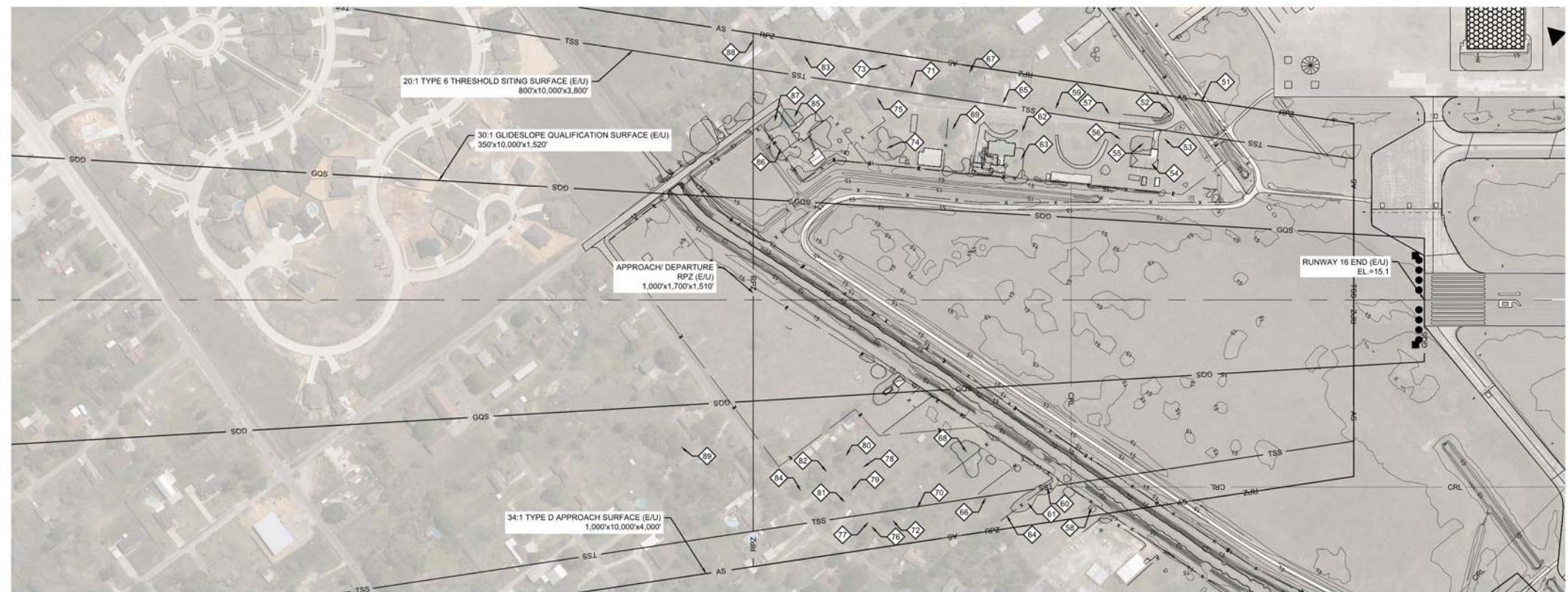
INNER PORTION
OF THE
APPROACH
SURFACE -
RUNWAY 30

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

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DRAWING NUMBER
IPASD-30

SHEET
NUMBER
6



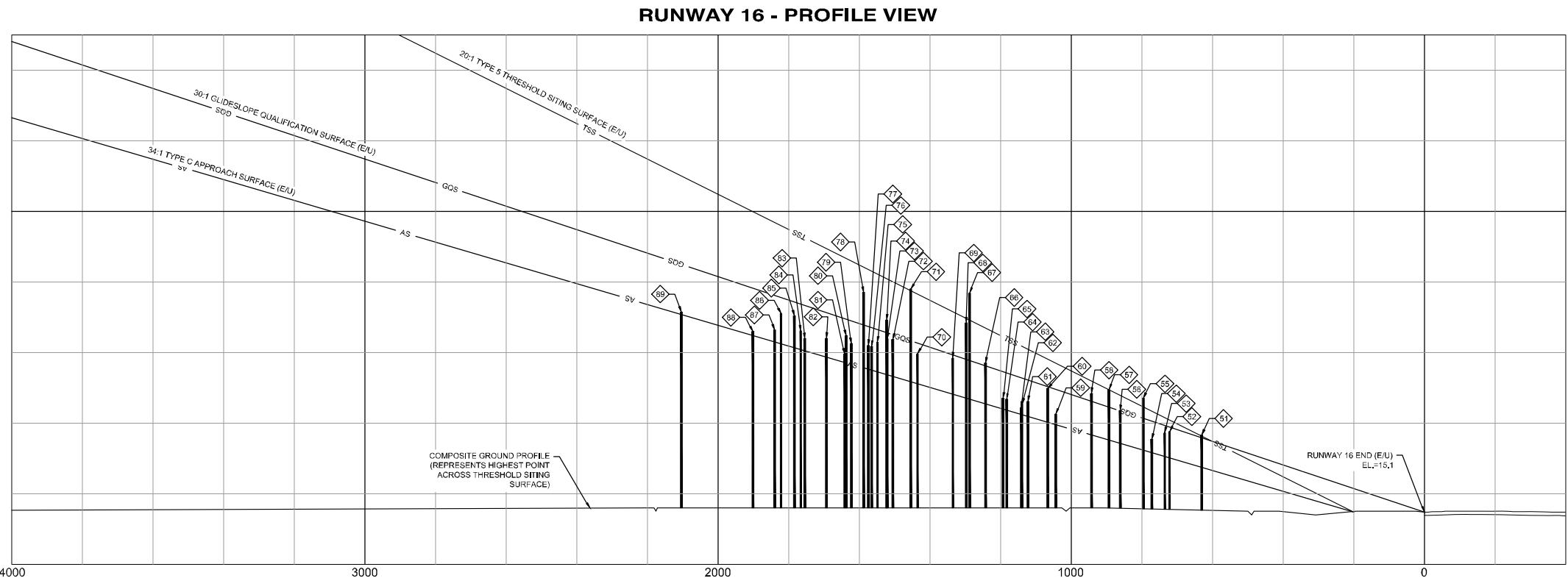
AIRPORT REFERENCE POINT COORDINATES ARE N49°38' VERTICAL CONTROL DATUM NAVD88 USED FOR ALL ELEVATIONS

NOTES:

1. SEE SHEET 9 - INNER APPROACH OBSTRUCTION TABLES FOR RUNWAY 34 INNER APPROACH OBSTRUCTION/DISPOSITION TABLE.
2. AERIAL PHOTOGRAPH AND OBSTRUCTION SURVEY PROVIDED BY SAM, INC. JULY 2013.

 REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY



LEGEND

ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 15'	BRL 35'
AIRPORT PROPERTY LINE	—	—
FENCE	X	XX
AIRFIELD PAVEMENT	—	—
PAVEMENT REMOVAL	—	—
BEACON	★	★
FUEL STORAGE AND PUMPS	—	SAME
BUILDINGS - ON AIRPORT	RPZ	RPZ(U)
APPROACH SURFACE	AS	AS(U)
THRESHOLD SITING SURFACE	TSS	TSS(U)
GLIDEPATH QUALIFICATION SURFACE	GOS	GOS(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	—	SAME
GROUND CONTOURS	680	SAME
PRECISION APPROACH PATH INDICATOR (PAPI)	■■■■	■■■■
THRESHOLD LIGHTS	●●●●	○○○○
RUNWAY END IDENTIFIER LIGHTS (REILS)	—	—
HOLDLINES & SIGNS	—	—
AIRPORT REFERENCE POINT (ARP)	—	CR'L
CROP LINE	—	SAME
VEGETATION	—	SAME
FLOW LINE	—	SAME
AVIGATION EASEMENT	—	—
LIGHT POLE	—	SAME
UTILITY POLE	—	SAME
ELECTRICAL UTILITY LINE	E	SAME

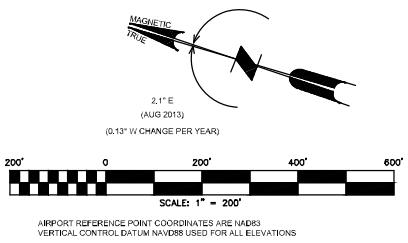
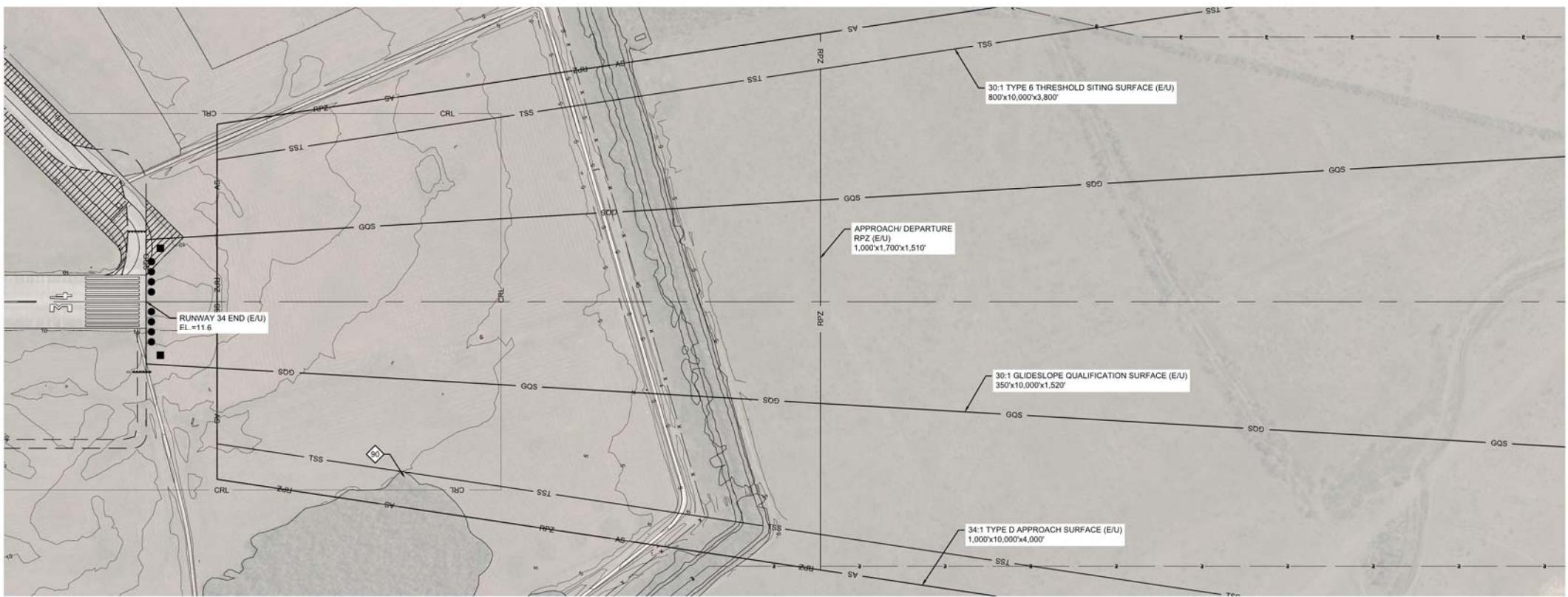
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 JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS
 AIRPORT LAYOUT PLAN

 INNER PORTION
 OF THE
 APPROACH
 SURFACE -
 RUNWAY 16

 JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

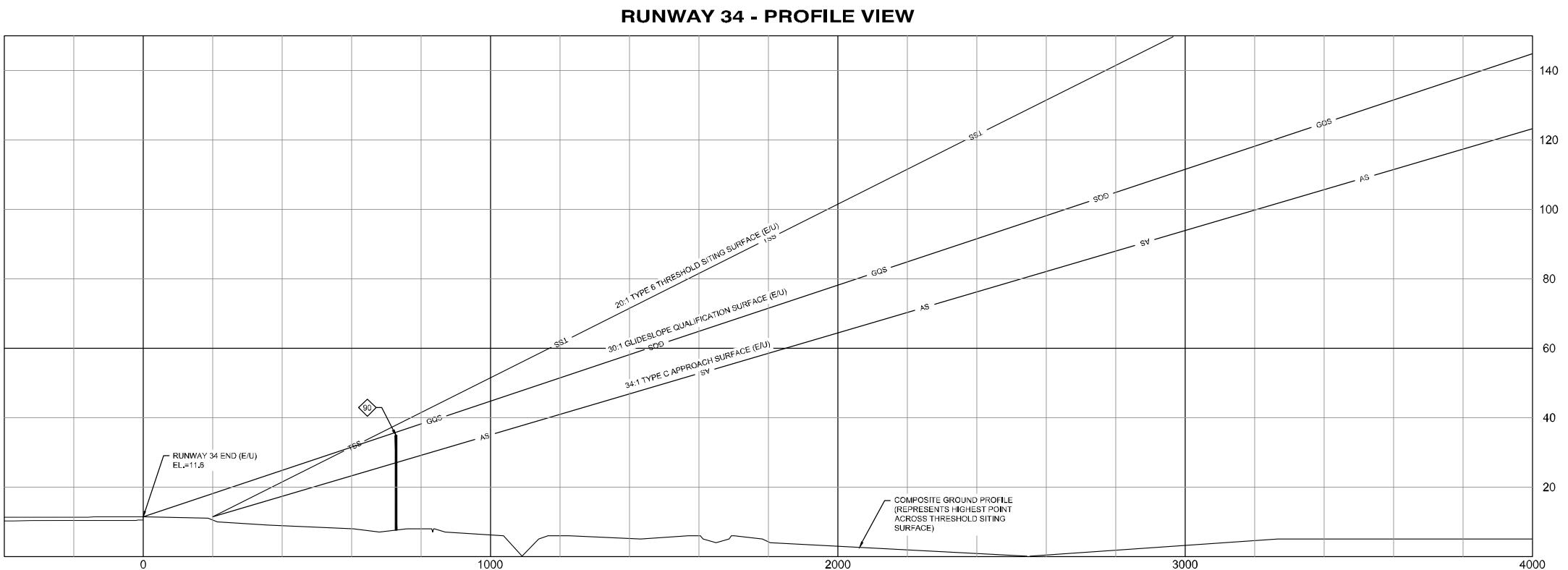
 DRAWING NUMBER
 IPASD-16
 SHEET
 NUMBER
 7



NOTES:
 1. SEE SHEET 9 - INNER APPROACH OBSTRUCTION TABLES FOR RUNWAY 34 INNER APPROACH OBSTRUCTION/DISPOSITION TABLE.
 2. AERIAL PHOTOGRAPH AND OBSTRUCTION SURVEY PROVIDED BY SAM, INC. JULY 2013.

 REGISTRATION NO.
 F-5713

REV.	DATE	DESCRIPTION	BY



LEGEND

ITEM	EXISTING	ULTIMATE
BUILDING RESTRICTION LINE	BRL 15'	BRL 35'
AIRPORT PROPERTY LINE	RPZ	RPZ(U)
FENCE	X	XX
AIRFIELD PAVEMENT		
PAVEMENT REMOVAL		
BEACON	*	*
FUEL STORAGE AND PUMPS		SAME
BUILDINGS - ON AIRPORT	RPZ	RPZ(U)
APPROACH SURFACE	AS	AS(U)
THRESHOLD SITING SURFACE	TSS	TSS(U)
GLIDEPATH QUALIFICATION SURFACE	GOS	GOS(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE		SAME
GROUND CONTOURS	680	SAME
PRECISION APPROACH PATH INDICATOR (PAPI)	■■■■	■■■■
THRESHOLD LIGHTS	●●●●	○○○○
RUNWAY END IDENTIFIER LIGHTS (REILS)	■■	□□
HOLDLINES & SIGNS	—	—
AIRPORT REFERENCE POINT (ARP)	●	⊕
CROP LINE	○○○○	CR'L
VEGETATION	○○○○	SAME
FLOW LINE	...	SAME
AVIGATION EASEMENT	■■■■	XXXXXX
LIGHT POLE	*	SAME
UTILITY POLE	—	SAME
ELECTRICAL UTILITY LINE	E	SAME

 JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS
 AIRPORT LAYOUT PLAN

 INNER PORTION
 OF THE
 APPROACH
 SURFACE -
 RUNWAY 34

 JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

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 SCALES ACCORDINGLY.

 DRAWING NUMBER
IPASD-34

 SHEET
8

RUNWAY 12 OBSTRUCTION/DISPOSITION TABLE					
DESCRIPTION	TOP ELEVATION	SURFACE PENETRATED	PENETRATION IN FEET	LOCATION	DISPOSITION
1 FENCE	21.3'	AS(E/U)	6.9'	198.1' FM RWY END / 469.6' RIGHT	NONE - CLEAR OF TSS
2 FENCE	22.0'	AS(E/U)	6.9'	233.4' FM RWY END / 464.1' RIGHT	NONE - CLEAR OF TSS
3 FENCE	22.6'	AS(E/U)	6.6'	280.1' FM RWY END / 458.3' RIGHT	NONE - CLEAR OF TSS
4 FENCE	22.7'	AS(E/U)	5.9'	320.3' FM RWY END / 452.8' RIGHT	NONE - CLEAR OF TSS
5 TREE(S)	32.1'	AS(E/U)	8.1'	678.3' FM RWY END / 502.5' LEFT	NONE - CLEAR OF TSS
6 TREE(S)	32.2'	AS(E/U)	5.0'	839.0' FM RWY END / 502.9' LEFT	NONE - CLEAR OF TSS
7 TREE(S)	40.7'	AS(E/U)	11.8'	924.5' FM RWY END / 537.4' RIGHT	NONE - CLEAR OF TSS
8 TREE(S)	36.5'	AS(E/U)	7.0'	954.3' FM RWY END / 505.5' LEFT	NONE - CLEAR OF TSS
9 TREE(S)	52.9'	AS(E/U)	21.8'	1,036' FM RWY END / 632.5' RIGHT	NONE - CLEAR OF TSS
10 TREE(S)	52.5'	TSS(E/U)	13.0'	1,051.8' FM RWY END / 505.5' RIGHT	NONE - CLEAR OF GGS*
11 TREE(S)	62.4'	TSS(E/U)	18.4'	1,204.9' FM RWY END / 510.8' RIGHT	NONE - CLEAR OF GGS*
12 TREE(S)	49.8'	AS(E/U)	12.0'	1,370.0' FM RWY END / 540.3' LEFT	NONE - CLEAR OF TSS
13 TREE(S)	49.3'	AS(E/U)	11.4'	1,374.7' FM RWY END / 620.6' RIGHT	NONE - CLEAR OF TSS
14 TREE(S)	49.7'	AS(E/U)	10.7'	1,4298.4' FM RWY END / 586.2' LEFT	NONE - CLEAR OF TSS
15 TREE(S)	61.1'	TSS(E/U)	7.7'	1,526.9' FM RWY END / 528.0' LEFT	NONE - CLEAR OF GGS*
16 TREE(S)	54.6'	TSS(E/U)	0.9'	1,534.9' FM RWY END / 453.4' LEFT	NONE - CLEAR OF GGS*
17 TREE(S)	56.4'	TSS(E/U)	2.5'	1,543.5' FM RWY END / 574.0' LEFT	NONE - CLEAR OF GGS*
18 TREE(S)	60.5'	TSS(E/U)	5.9'	1,556.4' FM RWY END / 482.1' RIGHT	NONE - CLEAR OF GGS*
19 TREE(S)	56.9'	TSS(E/U)	2.0'	1,576.1' FM RWY END / 397.8' LEFT	NONE - CLEAR OF GGS*
20 TREE(S)	56.1'	TSS(E/U)	3.1'	1,579.3' FM RWY END / 562.0' LEFT	NONE - CLEAR OF GGS*
21 TREE(S)	61.6'	TSS(E/U)	6.3'	1,589.9' FM RWY END / 583.4' RIGHT	NONE - CLEAR OF GGS*
22 TREE(S)	70.4'	AS(E/U)	28.1'	1,594.6' FM RWY END / 633.9' LEFT	NONE - CLEAR OF TSS
23 TREE(S)	63.1'	TSS(E/U)	3.2'	1,745.0' FM RWY END / 287.2' LEFT	NONE - CLEAR OF GGS*
24 TREE(S)	61.7'	AS(E/U)	15.2'	1,808.0' FM RWY END / 339.9' LEFT	NONE - CLEAR OF TSS
25 TREE(S)	47.9'	AS(E/U)	0.9'	1,827.5' FM RWY END / 301.4' LEFT	NONE - CLEAR OF TSS
26 TREE(S)	50.3'	AS(E/U)	3.2'	1,835.1' FM RWY END / 295.4' LEFT	NONE - CLEAR OF TSS
27 TREE(S)	68.4'	TSS(E/U)	5.5'	1,849.1' FM RWY END / 420.3' LEFT	NONE - CLEAR OF GGS*
28 TREE(S)	62.1'	AS(E/U)	13.7'	1,899.8' FM RWY END / 545.1' LEFT	NONE - CLEAR OF TSS
29 TREE(S)	60.0'	AS(E/U)	11.4'	1,904.8' FM RWY END / 291.4' RIGHT	NONE - CLEAR OF TSS
30 TREE(S)	59.9'	AS(E/U)	11.4'	1,906.6' FM RWY END / 301.2' RIGHT	NONE - CLEAR OF TSS
31 TREE(S)	52.1'	AS(E/U)	3.3'	1,919.3' FM RWY END / 270.5' LEFT	NONE - CLEAR OF TSS
32 TREE(S)	72.9'	TSS(E/U)	6.9'	1,954.5' FM RWY END / 384.6' RIGHT	NONE - CLEAR OF GGS*
33 TREE(S)	66.3'	AS(E/U)	14.8'	2,056.0' FM RWY END / 585.4' RIGHT	NONE - CLEAR OF TSS
34 TREE(S)	57.4'	AS(E/U)	3.7'	2,164.1' FM RWY END / 258.2' LEFT	NONE - CLEAR OF TSS
35 TREE(S)	63.8'	AS(E/U)	4.5'	2,438.4' FM RWY END / 237.3' RIGHT	NONE - CLEAR OF TSS
36 TREE(S)	70.9'	AS(E/U)	2.2'	2,915.0' FM RWY END / 387.1' RIGHT	NONE - CLEAR OF TSS
37 TREE(S)	72.1'	AS(E/U)	1.9'	2,986.9' FM RWY END / 281.6' RIGHT	NONE - CLEAR OF TSS
38 TREE(S)	78.2'	AS(E/U)	2.2'	3,277.5' FM RWY END / 98.5' RIGHT	NONE - CLEAR OF TSS
39 TREE(S)	56.7'	AS(E/U)	2.3'	1,174.9' FM RWY END / 623.1' RIGHT	NONE - CLEAR OF TSS
40 TREE(S)	52.2'	AS(E/U)	0.7'	2,053.7' FM RWY END / 191.0' RIGHT	NONE - CLEAR OF TSS

ALL CLEARANCES HAVE BEEN ADJUSTED FOR HEIGHT OF NATURAL OBJECTS, AND 10' FOR PRIVATE ROADS, 15' FOR PUBLIC ROADS,
17' FOR INTERSTATE HIGHWAYS, AND 23' FOR RAILROADS

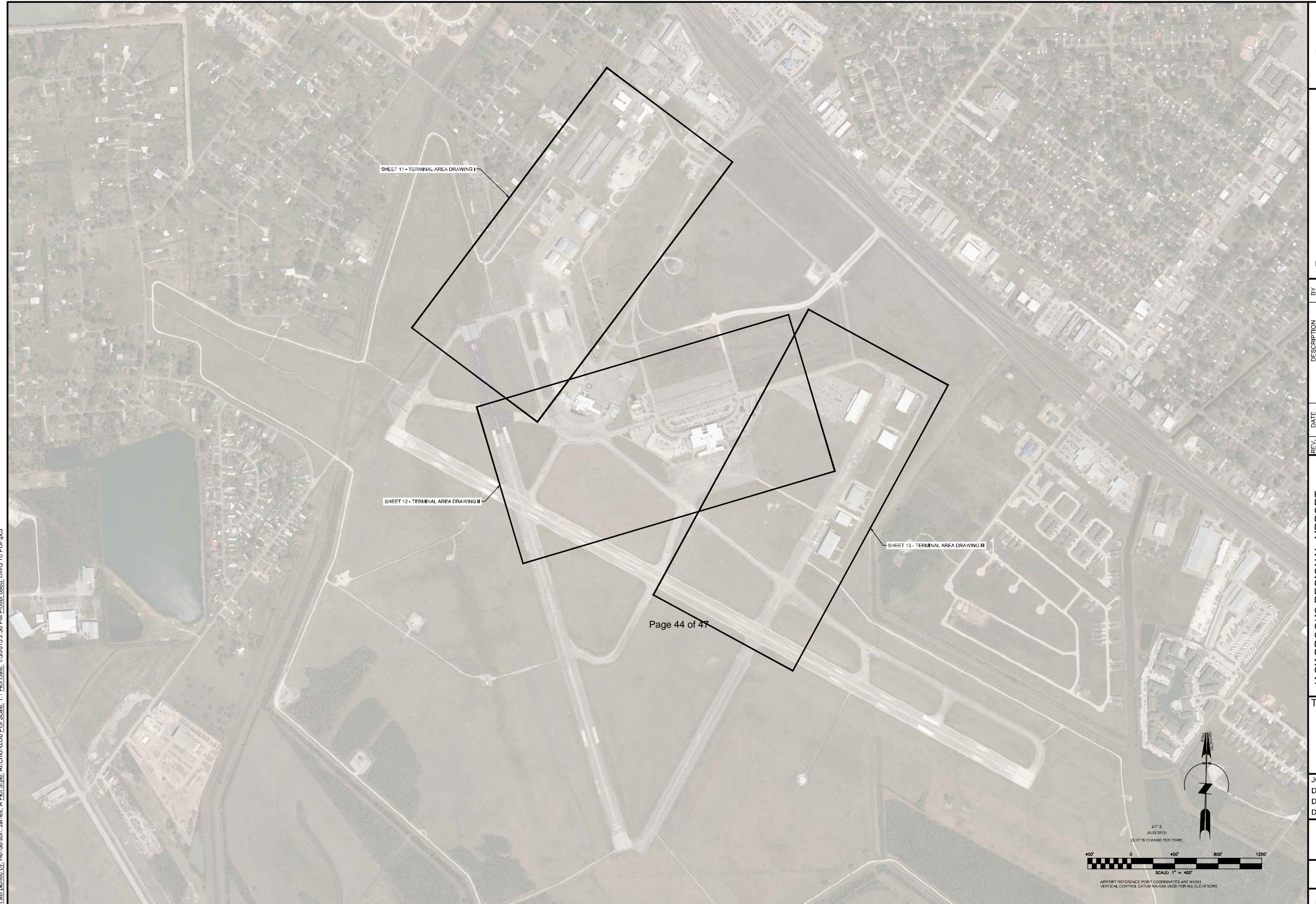
*TREES TO BE TOPPED / REMOVED

RUNWAY 30 OBSTRUCTION/DISPOSITION TABLE					
DESCRIPTION	TOP ELEVATION	SURFACE PENETRATED	PENETRATION IN FEET	LOCATION	DISPOSITION
1 TREE(S)	39.4'	AS(E/U)	10.4'	971.8' FM RWY END / 469.7' RIGHT	NONE - CLEAR OF TSS
2 TREE(S)	36.3'	AS(E/U)	6.5'	997.4' FM RWY END / 546.6' RIGHT	NONE - CLEAR OF TSS
3 TREE(S)	43.9'	AS(E/U)	13.2'	1,028.3' FM RWY END / 508.3' RIGHT	NONE - CLEAR OF TSS
4 TREE(S)	36.8'	AS(E/U)	3.8'	1,107.5' FM RWY END / 299.2' RIGHT	NONE - CLEAR OF TSS
5 TREE(S)	40.8'	AS(E/U)	6.3'	1,157.8' FM RWY END / 400.6' RIGHT	NONE - CLEAR OF TSS
6 TREE(S)	42.0'	AS(E/U)	6.7'	1,186.4' FM RWY END / 476.8' RIGHT	NONE - CLEAR OF TSS
7 TREE(S)	38.3'	AS(E/U)	2.6'	1,200.5' FM RWY END / 530.4' RIGHT	NONE - CLEAR OF TSS
8 TREE(S)	43.2'	AS(E/U)	7.2'	1,209.0' FM RWY END / 291.9' RIGHT	NONE - CLEAR OF TSS
9 TREE(S)	41.0'	AS(E/U)	4.3'	1,232.8' FM RWY END / 375.7' RIGHT	NONE - CLEAR OF TSS
10 TREE(S)	39.9'	AS(E/U)	0.8'	1,313.5' FM RWY END / 238.0' RIGHT	NONE - CLEAR OF TSS

ALL CLEARANCES HAVE BEEN ADJUSTED FOR HEIGHT OF NATURAL OBJECTS, AND 10' FOR PRIVATE ROADS, 15' FOR PUBLIC ROADS,
17' FOR INTERSTATE HIGHWAYS, AND 23' FOR RAILROADS

*TREES TO BE TOPPED / REMOVED

RUNWAY 16 OBSTRUCTION/DISPOSITION TABLE					
DESCRIPTION	TOP ELEVATION	SURFACE PENETRATED	PENETRATION IN FEET	LOCATION	DISPOSITION
1 POLE	36.6'	AS(E/U)	9.2'	630.6' FM RWY END / 561.8' LEFT	NONE - CLEAR OF TSS
2 POLE	37.6'	AS(E/U)	7.5'	721.5' FM RWY END / 531.9' LEFT	NONE - CLEAR OF TSS
3 POLE	37.2'	AS(E/U)	6.7'	735.1' FM RWY END / 456.3' LEFT	NONE - CLEAR OF TSS
4 BUILDING	35.4'	AS(E/U)	3.8'	771.7' FM RWY END / 384.8' LEFT	NONE - CLEAR OF TSS
5 TREE(S)	47.0'	TSS(E/U)	2.4'	795.7' FM RWY END / 443.5' LEFT	NONE - CLEAR OF GGS*
6 TREE(S)	43.5'	AS(E/U)	9.3'	860.6' FM RWY END / 454.9' LEFT	NONE - CLEAR OF TSS
7 POLE	49.4'	AS(E/U)	14.2'	892.7' FM RWY END / 527.2' LEFT	NONE - CLEAR OF TSS
8 TREE(S)	48.4'	AS(E/U)	11.8'	942.6' FM RWY END / 585.2' RIGHT	NONE - CLEAR OF TSS
9 POLE	42.5'	AS(E/U)	2.9'	1,043.4' FM RWY END / 543.0' LEFT	NONE - CLEAR OF TSS
10 TREE(S)	49.8'	AS(E/U)	9.5'	1,066.5' FM RWY END / 532.7' RIGHT	NONE - CLEAR OF TSS
11 TREE(S)	46.1'	AS(E/U)	4.2'	1,122.5' FM RWY END / 576.1' RIGHT	NONE - CLEAR OF TSS
12 TREE(S)	45.4'	AS(E/U)	3.0'	1,138.9' FM RWY END / 476.8' LEFT	NONE - CLEAR OF TSS
13 TREE(S)	44.4'	AS(E/U)	1.9'	1,141.0' FM RWY END / 389.9' LEFT	NONE - CLEAR OF TSS
14 TREE(S)	46.7'	AS(E/U)	3.0'	1,182.6' FM RWY END / 610.8' RIGHT	NONE - CLEAR OF TSS
15 POLE	47.0'	AS(E/U)	3.0'	1,192.5' FM RWY END / 557.6' LEFT	NONE - CLEAR OF TSS
16 TREE(S)	57.0'	AS(E/U)	11.6'	1,242.6' FM RWY END / 561.4' RIGHT	NONE - CLEAR OF TSS
17 TREE(S)	76.8'	AS(E/U)	30.0'	1,287.5' FM RWY END / 644.9' LEFT	NONE - CLEAR OF TSS
18 TREE(S)	68.3'	AS(E/U)	21.3'	1,297.0' FM RWY END / 581.4' RIGHT	NONE - CLEAR OF TSS
19 TREE(S)	58.4'	AS(E/U)	10.2'	1,336.1' FM RWY END / 483.9' LEFT	NONE - CLEAR OF TSS
20 TREE(S)	59.6'	AS(E/U)	8.5'	1,435.1' FM RWY END / 588.6' RIGHT	NONE - CLEAR OF TSS
21 TREE(S)	77.9'	AS(E/U)	26.2'	1,454.0' FM RWY END / 602.4' LEFT	NONE - CLEAR OF TSS
22 TREE(S)	63.7'	AS(E/U)	10.5'	1,504.6' FM RWY END / 625.0' RIGHT	NONE - CLEAR OF TSS
23 TREE(S)	63.8'	AS(E/U)	10.2'	1,521.7' FM RWY END / 666.3' LEFT	NONE - CLEAR OF TSS
24 TREE(S)	69.1'	AS(E/U)	15.4'	1,522.1' FM RWY END / 429.4' LEFT	NONE - CLEAR OF TSS
25 TREE(S)	62.9'	AS(E/U)	8.5'	1,548.4' FM RWY END / 572.1' LEFT	NONE - CLEAR OF TSS
26 TREE(S)	61.7'	AS(E/U)	6.8'	1,564.7' FM RWY END / 642.5' RIGHT	NONE - CLEAR OF TSS
27 TREE(S)	62.0'	AS(E/U)	6.8'	1,574.7' FM RWY END / 630.3' RIGHT	NONE - CLEAR OF TSS
28 TREE(S)	77.0'	AS(E/U)	21.4'	1,588.0' FM RWY END / 473.8' RIGHT	NONE - CLEAR OF TSS
29 TREE(S)	62.5'	AS(E/U)	5.9'	1,622.3' FM RWY END / 539.4' RIGHT	NONE - CLEAR OF TSS
30 TREE(S)	64				



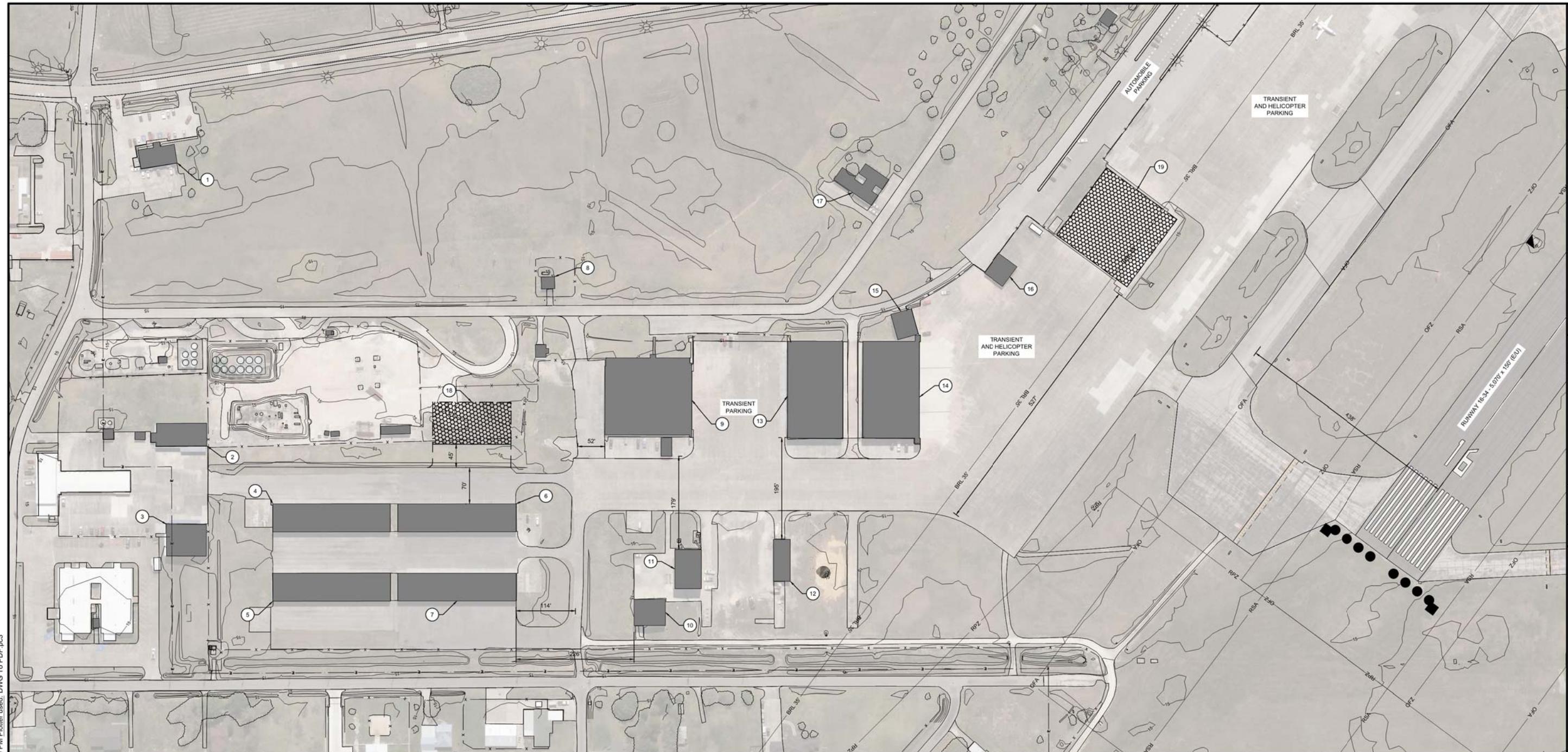
DESCRIPTION	BY

 JACK BROOKS REGIONAL AIRPORT
 BEAUMONT/PORT ARTHUR, TEXAS
 AIRPORT LAYOUT PLAN

 TERMINAL AREA
 DRAWING I
 JOB NO.: 12121740
 DATE: JAN, 2015
 DESIGNED BY: PLH
 DRAWN BY: JAH

 BAR IS ONE INCH ON
 ORIGINAL DRAWING
 IF NOT ONE INCH ON
 THIS SHEET, ADJUST
 SCALES ACCORDINGLY.

 DRAWING NUMBER
 TAD-1

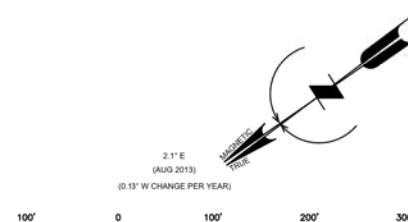
 SHEET
 NUMBER 11

LEGEND

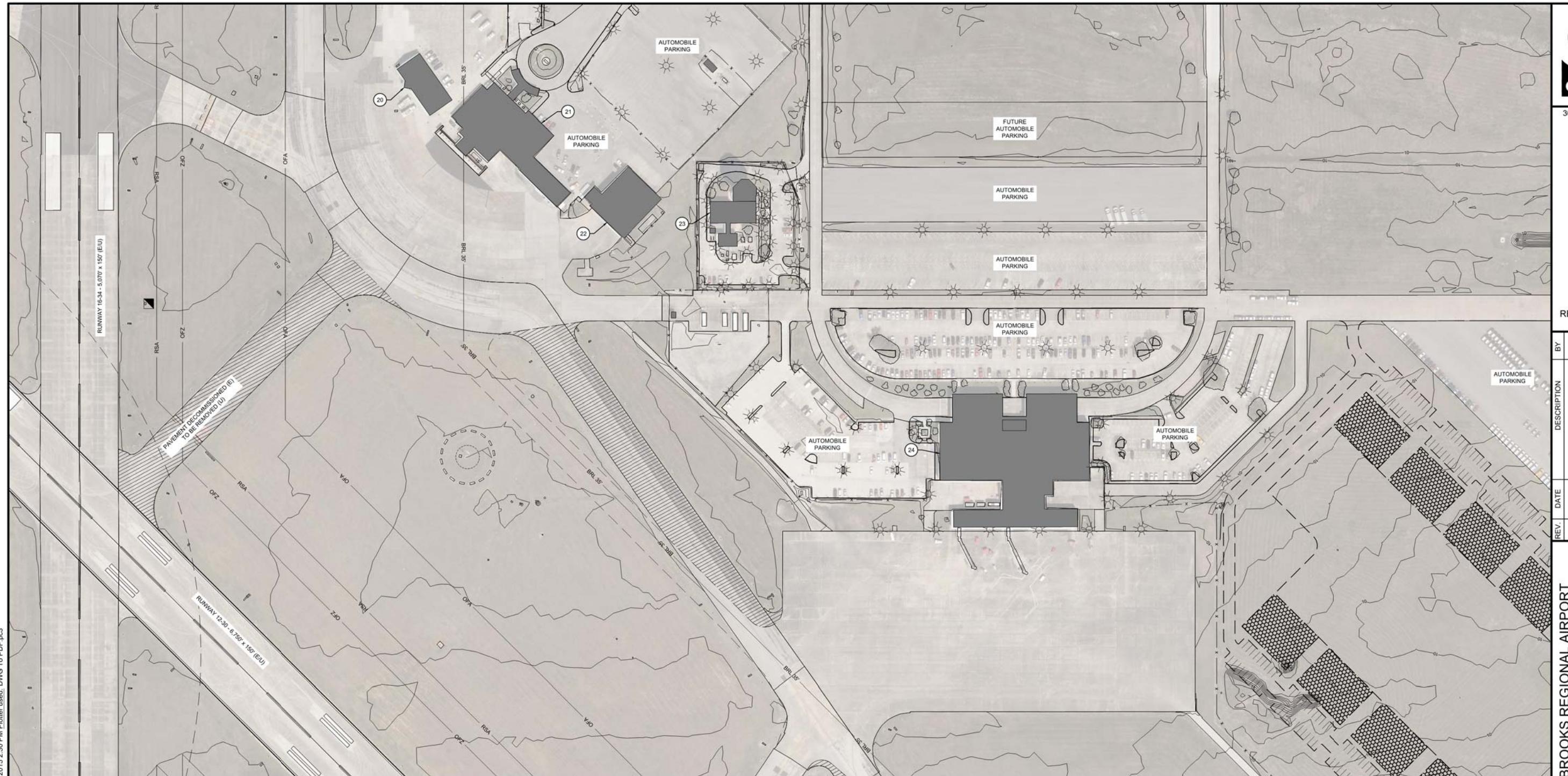
ITEM	INITIAL	ULTIMATE
BUILDING RESTRICTION LINE	BRL 35'	BRL 35'
AIRPORT PROPERTY LINE	P(U)	XX
FENCE	X	XX
AIRFIELD PAVEMENT		
PAVEMENT REMOVAL		
BEACON	★	★
FUEL STORAGE AND PUMPS		SAME
BUILDINGS - ON AIRPORT	RPZ	RPZ(U)
RUNWAY PROTECTION ZONE		
LIGHTED WIND CONE & SEGMENTED CIRCLE	○	○
HOLDLINES & SIGNS	—	■■■■
AIRPORT REFERENCE POINT (ARP)	●	⊕

EXISTING BUILDING TABLE

NO.	STRUCTURE	TOP ELEVATION
(1)	JEFFERSON COUNTY TAX OFFICE	26.00'
(2)	MOSQUITO CONTROL HANGAR	31.13'
(3)	MOSQUITO CONTROL HANGAR	34.22'
(4)	T-HANGAR	28.69'
(5)	T-HANGAR	28.58'
(6)	T-HANGAR	28.77'
(7)	T-HANGAR	29.15'
(8)	RENTAL CAR PREP	22.91'
(9)	HANGAR #4	29.70'
(10)	AIRCRAFT EQUIPMENT STORAGE, BUILDING	31.39'
(11)	MAINTENANCE BUILDING	39.10'
(12)	MAINTENANCE POLE BARN	26.89'
(13)	HANGAR #3	36.33'
(14)	HANGAR #2	36.09'
(15)	AIRPORT COMPUTER SHOP	31.55'
(16)	OFFICE BUILDING	27.91'
(17)	AIRPORT MANAGEMENT	25.15'
(18)	150' x 80' HANGAR	TBD
(19)	160' x 160' HANGAR	TBD

ALL BUILDINGS ARE CLEAR OF FAR PART 77 AIRSPACE SURFACES.

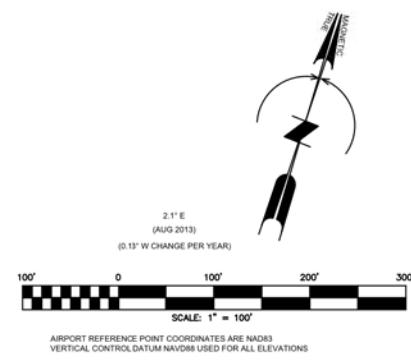

 100' 0 100' 200' 300'
 SCALE: 1" = 100'
 AIRPORT REFERENCE POINT COORDINATES ARE NAD83
 VERTICAL CONTROL DATUM NAVD88 USED FOR ALL ELEVATIONS



LEGEND		
ITEM	INITIAL	ULTIMATE
BUILDING RESTRICTION LINE	BRL 35'	BRL 35'
AIRPORT PROPERTY LINE	R	P(U)
FENCE	X	XX
AIRFIELD PAVEMENT	████████	████████
PAVEMENT REMOVAL	████████	████████
BEACON	★	★
FUEL STORAGE AND PUMPS	□	SAME
BUILDINGS - ON AIRPORT	████████	████████
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	○	○
HOLDLINES & SIGNS	—	—
AIRPORT REFERENCE POINT (ARP)	●	○

EXISTING BUILDING TABLE		
NO.	STRUCTURE	TOP ELEVATION
(20)	ARFF STATION	35.13'
(21)	EXECUTIVE TERMINAL	52.01'
(22)	HANGAR #6	34.40'
(23)	AIR TRAFFIC CONTROL TOWER	108.13'
(24)	AIR CARRIER PASSENGER TERMINAL	35.76'

ALL BUILDINGS ARE CLEAR OF FAR PART 77 AIRSPACE SURFACES.


 100' 0 100' 200' 300'
 SCALE: 1" = 100'

 AIRPORT REFERENCE POINT COORDINATES ARE NAD83
 VERTICAL CONTROL DATUM NAVD88 USED FOR ALL ELEVATIONS



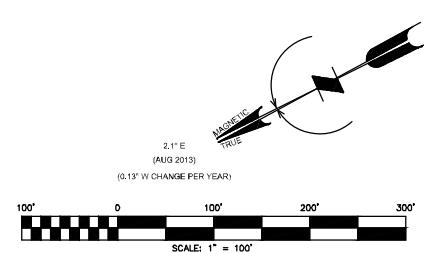
LEGEND

ITEM	INITIAL	ULTIMATE
BUILDING RESTRICTION LINE	BRL 35'	BRL 35'
AIRPORT PROPERTY LINE	P	P(U)
FENCE	X	XX
AIRFIELD PAVEMENT	██████████	██████████
PAVEMENT REMOVAL	██████████	██████████
BEACON	★	★
FUEL STORAGE AND PUMPS	□	SAME
BUILDINGS ON AIRPORT	████████████████	████████████████
RUNWAY PROTECTION ZONE	RPZ	RPZ(U)
LIGHTED WIND CONE & SEGMENTED CIRCLE	○	○
HOLDLINES & SIGNS	—	■■■■■
AIRPORT REFERENCE POINT (ARP)	●	⊕

EXISTING BUILDING TABLE

NO.	STRUCTURE	TOP ELEVATION
(25)	COUNTY SHERIFF'S DEPARTMENT	25.56'
(25)	HANGAR 7	30.66'
(27)	HANGAR 5	18.65'
(28)	PRIVATE - LAND LEASE HANGAR	19.78'
(29)	PRIVATE - LAND LEASE HANGAR	29.78'
(30)	PRIVATE - LAND LEASE HANGAR	42.88'
(31)	120' x 100' CORPORATE HANGAR	TBD
(32)	150' x 150' CORPORATE HANGAR	TBD
(33)	120' x 90' CORPORATE HANGAR	TBD

ALL BUILDINGS ARE CLEAR OF FAR PART 77 AIRSPACE SURFACES.



100' 0 100' 200' 300'

SCALE: 1" = 100'

2,1° E (AUG 2013)
(0.13° W CHANGE PER YEAR)

MAGNETIC
TRUE

AIRPORT REFERENCE POINT COORDINATES ARE NAD83
VERTICAL CONTROL DATUM NAVD88 USED FOR ALL ELEVATIONS

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS

AIRPORT LAYOUT PLAN

TERMINAL AREA
DRAWING III

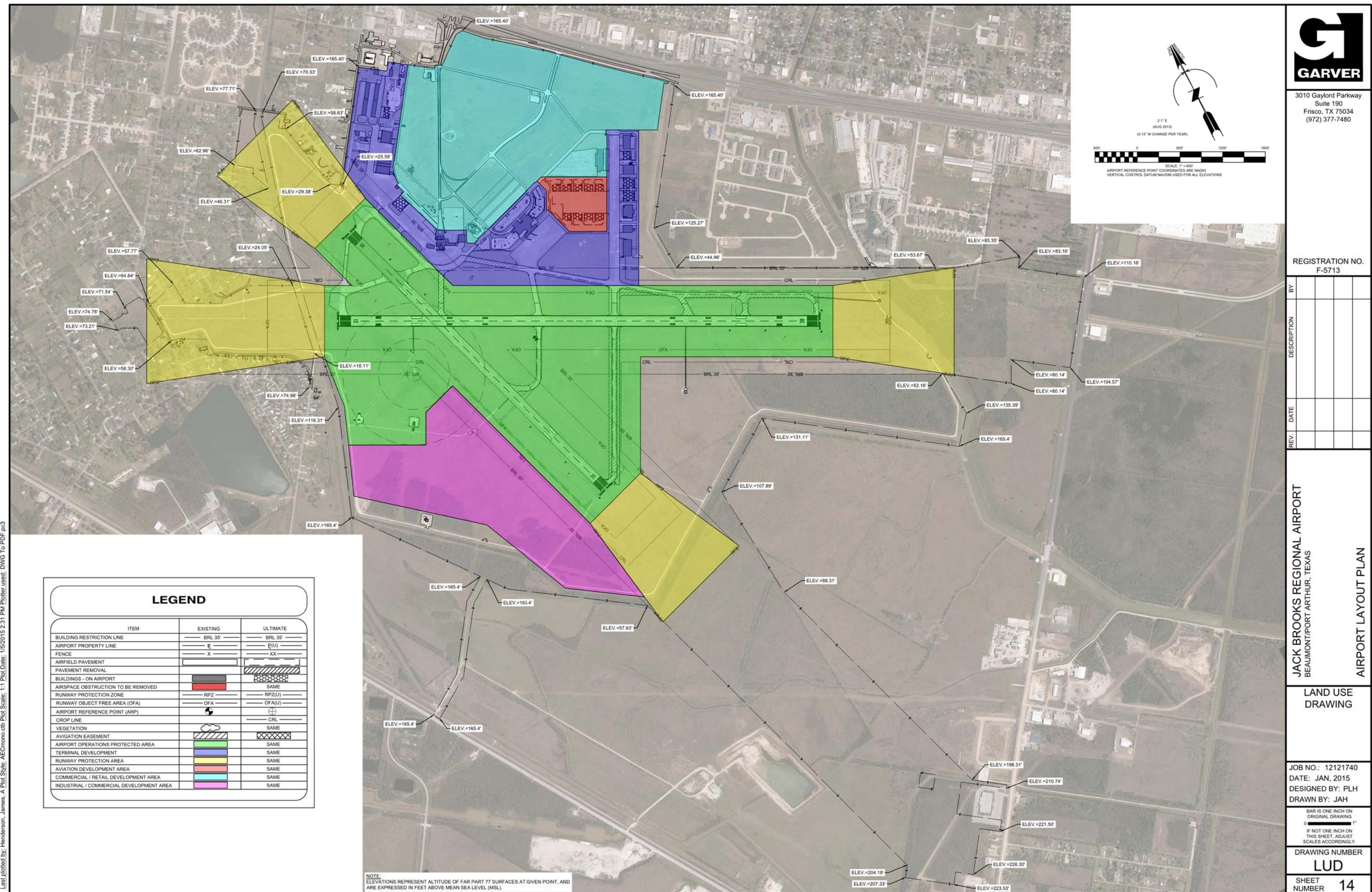
JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

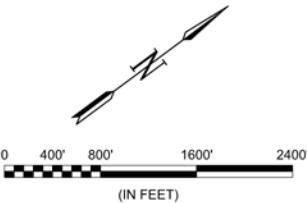
BAR IS ONE INCH OF
ORIGINAL DRAWING
0 1"

IF NOT ONE INCH OF
THIS SHEET, ADJUST
SCALES ACCORDINGLY.

DRAWING NUMBER
TAD-3

SHEET
NUMBER 13





3010 Gaylord Parkway
Suite 190
Frisco, TX 75034
(972) 377-7480

REGISTRATION NO.
F-5713

REV.	DATE	DESCRIPTION	BY

JACK BROOKS REGIONAL AIRPORT
BEAUMONT/PORT ARTHUR, TEXAS

AIRPORT
PROPERTY KEY
MAP

JOB NO.: 12121740
DATE: JAN, 2015
DESIGNED BY: PLH
DRAWN BY: JAH

BAR IS ONE INCH ON
ORIGINAL DRAWING
0 1"
IF NOT ONE INCH ON
THIS SHEET, ADJUST
SCALES ACCORDINGLY.

DRAWING NUMBER
APKM

sheet
number 15

File: L:\10120\12121740 - BPT ALP Update & Obstruction Survey\Drawings\BPT-ALP-14.dwg Last Save: 1/5/2015 1:27 PM Last saved by: Pihavener
Last plotted by: Henderson, James A Plot Style: AECmono ctb Plot Scale: 1:1 Plot Date: 1/5/2015 2:32 PM Plotter used: DWG To PDF.pc3









OWNERSHIP TABLE					
TRACT	GRANTOR	DATE	VOL/PAGE	DESCRIPTION	
1	FRANK E. RANDALL AND WIFE, MAY C. RANDALL	11/25/1957	VOL. 1091, PAGE 587	LOT 47 SHAMROCK ACRES	
2	ROY N. WOOD AND JOHNNIE WOOD	10/14/1957	VOL. 1087, PAGE 137	LOT 48 SHAMROCK ACRES	
3	J. H. KILBUCK AND WIFE, IRMA LEOMA KILBUCK	10/14/1957	VOL. 1087, PAGE 149	LOT 44 SHAMROCK ACRES	
4	DILBERT C. WHITEHEAD AND WIFE, MACLE WHITEHEAD	3/5/1957	VOL. 1104, PAGE 550	LOT 43 SHAMROCK ACRES	
5	EARL J. WIEGMANN AND ELIZABETH KEITH WIEGMANN	11/4/1957	VOL. 1088, PAGE 270	LOT 42 SHAMROCK ACRES	
6	HURSHEL A. HOYE AND EVELYN L. HOYE	6/5/1958	VOL. 1117, PAGE 459	PART OF LOT 41 SHAMROCK ACRES	
7	CARL R. BRADLEY AND MARIE BRADLEY	11/20/1957	VOL. 1091, PAGE 605	PART OF LOT 39 SHAMROCK ACRES	
8	MARY E. YOUNG, et al	8/16/1955	VOL. 993, PAGE 85	TRACT A-3, 1.08 ACRES	
9	MARY E. YOUNG, et al	8/16/1955	VOL. 993, PAGE 85	TRACT A-4, 0.032 ACRES	
10	MARY E. YOUNG, et al	8/16/1955	VOL. 993, PAGE 85	SEVERAGE TRACT 51.197 ACRES	
11	MARY E. YOUNG, et al	8/16/1955	VOL. 993, PAGE 85	TRACT A-1, 37.22 ACRES	
12	MARY E. YOUNG, et al	8/16/1955	VOL. 993, PAGE 85	TRACT A-2, 52.08 ACRES	
13	ROY C. WILSON AND WIFE, RUTH ANN WILSON	8/8/1955	VOL. 990, PAGE 73	TRACT G-1, 2.485 ACRES	
14	ROY C. WILSON AND WIFE, RUTH ANN WILSON	8/8/1955	VOL. 990, PAGE 73	TRACT G-2, 0.248 ACRES	
15	MARY E. YOUNG, et al	8/16/1955	VOL. 993, PAGE 85	TRACT C-1, 0.974 ACRES	
16	WILLIAM BOSTICK, et al	12/3/1970	VOL. 1663, PAGE 56	PART OF LOTS 16 & 17, BLOCK 7, HILLCREST SECOND ADDITION	
17	R. D. DAWSON	4/16/1956	VOL. 1023, PAGE 378	LOT 18, BLOCK 7, HILLCREST SECOND ADDITION	
18	RICHARD E. DOORNbos, et al	3/7/2001	CF#2001008588	PARCEL 58 4.286 ACRES	
19	C. DOORNbos	7/22/1941	VOL. 499, PAGE 136	144.86 ACRES, FIRST TRACT	
20	W. W. KYLE	2/10/1941	VOL. 499, PAGE 375	382.86 ACRES	
21	MARY E. YOUNG, et al	8/16/1955	VOL. 993, PAGE 85	TRACT C-2, 0.825 ACRES	
22	ESTATE OF P. H. WIESS	8/20/1941	VOL. 499, PAGE 376	102.48 ACRES	
23	MCFADDIN TRUST	2/9/1942	VOL. 505, PAGE 167	8.56 ACRES	
24	MARY E. YOUNG, et al	7/21/1941	VOL. 499, PAGE 138	31.57 ACRES, FIRST TRACT	
25	J. D. FORD AND WIFE, VERA FORD	4/27/1957	VOL. 1068, PAGE 582	17.183 ACRES	
26	J. D. FORD AND WIFE, VERA FORD	4/27/1957	VOL. 1068, PAGE 582	11.321 ACRES	
27	EMMETTE DARNELL AND WIFE, KATHERINE M. DARNELL		VOL. 1208, PAGE 290	0.50 ACRES	
28	CHARLES H. HOOKER AND BOBBIE HOOKER	12/26/1957	VOL. 1095, PAGE 139	PARCEL A	
29	CHARLES H. HOOKER AND BOBBIE HOOKER	12/26/1957	VOL. 1095, PAGE 139	PARCEL B	
30	C. DOORNbos	7/22/1941	VOL. 499, PAGE 136	185.12 ACRES, SECOND TRACT	
31	J. D. FORD AND WIFE, VERA FORD	6/15/1955	VOL. 1006, PAGE 273	TRACT 1, 3.68 ACRES	
32	J. D. FORD AND WIFE, VERA FORD	6/15/1955	VOL. 1006, PAGE 273	TRACT 2, 0.016 ACRES	
33	ESTATE OF P. H. WIESS	2/14/1956	VOL. 1018, PAGE 195	TRACT 14, 14.70 ACRES	
34	ESTATE OF P. H. WIESS	2/14/1956	VOL. 1018, PAGE 195	TRACT 14A, 0.90 ACRES	
35	MARY E. YOUNG, et al	7/21/1941	VOL. 499, PAGE 138	24.99 ACRES, SECOND TRACT	
36	VERA G. MCVEAN	7/21/1956	VOL. 1030, PAGE 89	PART OF LOTS 12 & 13, BLOCK 6, HILLCREST PLACE ADDITION	
37	J. D. FORD AND WIFE, VERA FORD	4/27/1957	VOL. 1068, PAGE 582	0.126 ACRES	
38	HARRY J. COMSTOCK AND WIFE, DELORES J. COMSTOCK	8/20/1969	VOL. 1609, PAGE 385	PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION	
39	GILBERT L. BUZZINGHAM	9/15/1969	VOL. 1610, PAGE 361	PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION	
40	FREDDIE R. DAVIS AND WIFE, MARGARET DAVIS	9/3/1969	VOL. 1610, PAGE 358	PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION	
41	ROBERT V. FAULK AND WIFE, SUSIE FAULK	9/2/1969	VOL. 1610, PAGE 345	PART OF LOT 6, BLOCK 8, HILLCREST SECOND ADDITION	
42	C. J. ARENAS AND WIFE, EVA ARENAS	8/26/1969	VOL. 1610, PAGE 351	PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION	
43	RUSSELL J. VAUGHAN AND WIFE, OLIVE VAUGHAN	9/3/1969	VOL. 1610, PAGE 353	PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION	
44	VIRGINIA BURTON MCINNIS	8/25/1969	VOL. 1610, PAGE 343	PART OF LOT 7, BLOCK 8, HILLCREST SECOND ADDITION	
45	ROBERT O. BEARDEN AND WIFE, RUBY M. BEARDEN	9/8/1969	VOL. 1611, PAGE 416	PART OF LOTS 6 & 7, BLOCK 8, HILLCREST SECOND ADDITION	
46	OLIVER C. LAWSON AND WIFE, EDITH L. LAWSON	8/25/1969	VOL. 1612, PAGE 281	PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION	
47	EDNA B. KING, et al	10/1/1969	VOL. 1614, PAGE 34	PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION	
48	EDNA B. KING, et al	10/1/1969	VOL. 1614, PAGE 24	PART OF LOT 6, BLOCK 8, HILLCREST SECOND ADDITION	
49	HENRY J. HOPKINS AND WIFE, JOANN HOPKINS	4/29/1970	VOL. 1636, PAGE 231	PART OF LOT 6, BLOCK 8, HILLCREST SECOND ADDITION	
50	EUGENE G. RASHETA AND WIFE, BEVERLY ANN RASHETA	4/22/1970	VOL. 1936, PAGE 233	PART OF LOTS 6 & 7, BLOCK 8, HILLCREST SECOND ADDITION	
51	BILLY M. BAILEY AND WIFE, CYNTHIA BAILEY	5/1/1970	VOL. 1636, PAGE 234	PART OF LOT 12, BLOCK 8, HILLCREST SECOND ADDITION	
52	ROBERT E. SMITH AND WIFE, ALBURTHA MAE SMITH	5/6/1970	VOL. 1637, PAGE 351	PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION	
53	JOHN E. BROADWAY AND WIFE, BETTYE L. BROADWAY	5/6/1970	VOL. 1637, PAGE 361	PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION	
54	MARJORIE ANN ARENDALE	5/6/1970	VOL. 1637, PAGE 358	PART OF LOTS 13 & 13, BLOCK 8, HILLCREST SECOND ADDITION	
55	EDWARD L. PULLIN AND WIFE, VIRGINIA PULLIN	5/12/1970	VOL. 1638, PAGE 174	PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION	
56	ROBERT H. BRADBERRY AND WIFE, MARION ELIZABETH BRADBERRY	5/12/1970	VOL. 1638, PAGE 176	PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION	
57	PORT ARTHUR SAVINGS AND LOAN ASSOCIATION	5/12/1970	VOL. 1638, PAGE 183	PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION	
58	CALVIN VINCENT AND WIFE, GERALDINE VINCENT	5/19/1970	VOL. 1640, PAGE 69	PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION	
59	CARLE FINLEY AND WIFE, FERN J. FINLEY	2/17/1972	VOL. 1722, PAGE 192	PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION	
60	ALTON WILLIAMS AND WIFE, BERTHA WILLIAMS	5/27/1970	VOL. 1040, PAGE 197	PART OF LOTS 5 & 6, BLOCK 8, HILLCREST SECOND ADDITION	
61	JUNIUS P. BARKELY AND WIFE, ELIE ANN BARKLEY	6/4/1970	VOL. 1641, PAGE 230	PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION	
62	JOHN K. SATTLER AND WIFE, GLORIA SATTLER	6/1/1970	VOL. 1641, PAGE 300	PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION	
63	VERNEST A. MILLER AND WIFE, ELLA MILLER	6/3/1970	VOL. 1642, PAGE 168	PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION	
64	C. R. TYREE AND WIFE, IRENE TYREE	6/15/1970	VOL. 1642, PAGE 170	PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION	
65	ALEXDORNE AND WIFE, MAXINE DORE	11/25/1970	VOL. 1662, PAGE 323	PART OF LOTS 6 & 7, BLOCK 8, HILLCREST SECOND ADDITION	
66	THOMAS H. WELLS AND WIFE, MARINELL WELLS	11/9/1970	VOL. 1662, PAGE 324	PART OF LOT 12, BLOCK 8, HILLCREST SECOND ADDITION	
67	MARY ELIZABETH HARLE, et al	1/27/1971	VOL. 1669, PAGE 70	PART OF LOT 4 & 5, BLOCK 14, RANGE "K" PALCO SUB.	
68	WILLIAM DOORNbos, et al	2/23/1971	VOL. 1671, PAGE 191	0.835 ACRE TRACT PART OF LOTS 8 & 9, BLOCK 8, HILLCREST SECOND ADDITION	
69	W. R. FRASIER AND WIFE, MARY AGNES FRASIER	2/7/1972	VOL. 1720, PAGE 118	PART OF LOT 7, BLOCK 8, HILLCREST SECOND ADDITION	
70	LAWRENCE T. APPLEGATE AND WIFE, NETA APPLEGATE	2/10/1972	VOL. 1721, PAGE 480	PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION	
71	ARVINA O'PRY	2/10/1972	VOL. 1721, PAGE 483	PART OF LOTS 6 & 7, BLOCK 8, HILLCREST SECOND ADDITION	
72	JOHN C. BROWN AND WIFE, CRYSTAL L. BROWN	2/16/1972	VOL. 1722, PAGE 178	PART OF LOT 13, BLOCK 8, HILLCREST SECOND ADDITION	
73	THOMAS E. EDGERLY, et al	2/8/1972	VOL. 1723, PAGE 142	TRACT ONE, PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION	
74	THOMAS E. EDGERLY, et al	2/8/1972	VOL. 1723, PAGE 142	TRACT TWO, PART OF LOTS 13 & 14, BLOCK 8, HILLCREST SECOND ADDITION	
75	VIRGINIA BURTON MCINNIS	3/22/1972	VOL. 1727, PAGE 157	PART OF LOT 12, BLOCK 8, HILLCREST SECOND ADDITION	
76	ROBERT T. MUNDAY AND PERCY ALEXANDER MARSHALL	9/23/1972	VOL. 1753, PAGE 146	24.440 ACRES, PART OF LOTS 1, 2 AND 3, BLOCK 18, RANGE "M" PALCO SUB.	
77	WILLIAM DOORNbos, et al	10/8/1973	VOL. 1806, PAGE 432	12.088 ACRES, PART OF LOTS 8, 9, 10 AND 11, BLOCK 8, HILLCREST SECOND ADDITION	
78	GORDON R. PATE AND J. H. ARMSTRONG	2/2/1976	VOL. 1919, PAGE 478	PART OF LOTS 12 & 13, BLOCK 8, HILLCREST SECOND ADDITION	
79	VIRGINIA BURTON MCINNIS, et al	10/29/1970	VOL. 1663, PAGE 60	0.943 ACRES PART OF LOT 5, BLOCK 8, HILLCREST SECOND ADDITION	
80	UNABLE TO LOCATE RECORDED INFORMATION AS TO OWNERS				



> 1 ASSOC CITY: BEAUMONT/PORT ARTHUR 4 STATE: TX
 > 2 AIRPORT NAME: JACK BROOKS RGNL
 3 CBD TO AIRPORT (NM): 09 SE 6 REGION/ADO: ASW/TEX
 LOC ID: BPT
 5 COUNTY: JEFFERSON TX
 7 SECT AERO CHT: HOUSTON

FAA SITE NR: 23416.*A

GENERAL

10 OWNERSHIP: PUBLIC
 > 11 OWNER: JEFFERSON COUNTY
 > 12 ADDRESS: 1149 PEARL ST 4TH FLOOR
 BEAUMONT, TX 77701
 > 13 PHONE NR: 409-835-8466
 > 14 MANAGER: ALEX RUPP
 > 15 ADDRESS: 5000 JERRY WARE DR SUITE 100
 BEAUMONT, TX 77705
 > 16 PHONE NR: 409-719-4900
 > 17 ATTENDANCE SCHEDULE:

ALL ALL ALL

SERVICES

> 70 FUEL: 100LL A
 > 71 AIRFRAME RPRS: MINOR
 > 72 PWR PLANT RPRS: MINOR
 > 73 BOTTLE OXYGEN: NONE
 > 74 BULK OXYGEN: LOW
 75 TSNT STORAGE: HGR, TIE
 76 OTHER SERVICES: AFRT, CHTR, INSTR, RNTL

BASED AIRCRAFT

90 SINGLE ENG: 60
 91 MULTI ENG: 8
 92 JET: 16
 TOTAL: 84
 93 HELICOPTERS: 5
 94 GLIDERS: 0
 95 MILITARY: 0
 96 ULTRA-LIGHT: 0

18 AIRPORT USE: PUBLIC
 19 ARPT LAT: 29-57-03.0000N ESTIMATED
 20 ARPT LONG: 094-01-14.5000W
 21 ARPT ELEV: 15.4 SURVEYED
 22 ACREAGE: 1,799
 > 23 RIGHT TRAFFIC: NO
 > 24 NON-COMM LANDING: NO
 25 NPIAS/FED AGREEMENTS: NGPY3
 > 26 FAR 139 INDEX: I AS 05/1973

FACILITIES

> 80 ARPT BCN: CG
 > 81 ARPT LGT SKED : SEE RMK
 BCN LGT SKED: SS-SR
 > 82 UNICOM: 122.950
 > 83 WIND INDICATOR: YES-L
 84 SEGMENTED CIRCLE: YES
 85 CONTROL TWR: YES
 86 FSS: MONTGOMERY
 87 FSS ON ARPT: NO
 88 FSS PHONE NR:
 89 TOLL FREE NR: 1-800-WX-BRIEF

OPERATIONS

100 AIR CARRIER: 21
 102 AIR TAXI: 1,929
 103 G A LOCAL: 3,841
 104 G A ITNRNT: 9,413
 105 MILITARY: 1,071
 TOTAL: 16,275

OPERATIONS FOR
 12 MONTHS
 ENDING: 12/31/2018

RUNWAY DATA

> 30 RUNWAY INDENT:
 > 31 LENGTH: 6,750
 > 32 WIDTH: 150
 > 33 SURF TYPE-COND: CONC-G
 > 34 SURF TREATMENT: GRVD
 35 GROSS WT: S 90.0
 36 (IN THSDS) D 170.0
 37 2D 230.0
 38 2D/2D2
 > 39 PCN: 37 /R/D/X/U

12/30	16/34
6,750	5,070
150	150
CONC-G	ASPH-CONC-F
GRVD	GRVD
90.0	70.0
170.0	90.0
230.0	145.0

26 /R/D/X/U

LIGHTING/APCH AIDS

> 40 EDGE INTENSITY: HIGH
 > 42 RWY MARK TYPE-COND: PIR - G / PIR - G
 > 43 VGS: / V4L
 44 THR CROSSING HGT / 54
 45 VISUAL GLIDE ANGLE: / 3.00
 > 46 CNTRLN-TDZ: - / -
 > 47 RVR-RVV: T - N / R -
 > 48 REIL: / Y
 > 49 APCH LIGHTS: MALS R /

HIGH	HIGH
PIR - G / PIR - G	NPI - G / NPI - G
/ V4L	V4L / V4L
/ 54	52 / 53
/ 3.00	3.00 / 3.00
- / -	- / -
T - N / R -	- / -
/ Y	Y / Y
MALS R /	/

OBSTRUCTION DATA

50 FAR 77 CATEGORY
 > 51 DISPLACED THR:
 > 52 CTLG OBSTN:
 > 53 OBSTN MARKED/LGTD:
 > 54 HGT ABOVE RWY END:
 > 55 DIST FROM RWY END:
 > 56 CNTRLN OFFSET:
 57 OBSTN CLNC SLOPE:
 58 CLOSE-IN OBSTN:

PIR / C	C / C
/	/
TREE / TREE	TREE /
/	/
46 / 41	48 /
1,041 / 1,024	761 /
618R / 529R	327L /
18:1 / 20:1	11:1 / 50:1
N / N	N / N

DECLARED DISTANCES

> 60 TAKE OFF RUN AVBL (TORA): 6,750 / 6,750
 > 61 TAKE OFF DIST AVBL (TODA): 6,750 / 6,750
 > 62 ACFT STOP DIST AVBL (ASDA): 6,675 / 6,750
 > 63 LNDG DIST AVBL (LDA): 6,675 / 6,750

6,750 / 6,750	5,070 / 5,070
6,750 / 6,750	5,070 / 5,070
6,675 / 6,750	5,070 / 5,070
6,675 / 6,750	5,070 / 5,070

(> ARPT MGR PLEASE ADVISE FSS IN ITEM 86 WHEN CHANGES OCCUR TO ITEMS PRECEDED BY >

> 110 REMARKS

A 033 RWY 16/34 FIRST 1000 FT RY 16 MUCH DARKER THAN REMAINDER OF RY.
 A 034 RY 16/34 FIRST 4271 FT OF RY 34 GRVD.
 A 039 RWY 16/34 FIRST 1,000 FEET OF RUNWAY 16 PCN REPORTED AS 28/F/D/X/U
 A 081 DUSK-DAWN. WHEN ATCT CLSD HIRL RY 12/30 & REIL RY 30; HIRL RY 16/34 & REIL RYS 16 & 34 PRESET MED INTST. ACTVT MALS R RY 12 - CTAF.
 A 110 THIS AIRPORT HAS BEEN SURVEYED BY THE NATIONAL GEODETIC SURVEY.
 A 110-001 BIRD ACTIVITY ON & INVOF ARPT.
 A 110-002 TWY HOLD SHORT SIGN FOR RWY 16/34 AND RWY 12/30 NOT LGTD. TWY C TWY LGT OTS. TWY C CLSD LGTD AND BARRICADED.
 A 110-003 FOR CD WHEN ATCT IS CLSD CTC HOUSTON APCH AT 281-443-5844 TO CNL IFR AT 281-443-5888.
 A 110-005 TWY G CLSD PERM.
 A 110-006 TWY H CLSD PERM.

111 INSPECTOR: (F)

112 LAST INSP: 01/17/2019

113 LAST INFO REQ:

Source: ACAIS FAA Airports				Commercial Service Airports (Rank Order) based on Calendar Year 2017 Enplanements					11/7/2018	
326 EA MD HGR	Hagerstown	Hagerstown Regional-Richard A Henson Field	P	N	25,584	27,274	-6.20%			
327 EA WV CKB	Clarksburg	North Central West Virginia	P	N	25,105	26,025	-3.54%			
328 GL MI CMX	Hancock	Houghton County Memorial Santa Maria Public/Capt G Allan Hancock Field	P	N	24,843	25,439	-2.34%			
329 WP CA SMX	Santa Maria	Pellston Regional Airport of Emmet County	P	N	24,527	40,120	-38.87%			
330 GL MI PLN	Pellston	Key Field	P	N	23,961	25,497	-6.02%			
331 SO MS MEI	Meridian	Jack Brooks Regional	P	N	23,923	26,359	-9.24%			
332 SW TX BPT	Beaumont	Rhinelander	P	N	23,767	26,518	-10.37%			
334 GL WI RHI	Rhinelander	Rhinelander-Oneida County	P	N	23,014	20,414	12.74%			
335 EA NY OGS	Ogdensburg	Ogdensburg International	P	N	22,957	10,281	123.30%			
336 AL AK PSG	Petersburg	Petersburg James A Johnson	P	N	22,915	21,896	4.65%			
337 GL WI EAU	Eau Claire	Chippewa Valley Regional	P	N	22,822	21,304	7.13%			
338 EA NY ART	Watertown	Watertown International	P	N	22,785	17,312	31.61%			
339 GL MI CIU	Sault Ste. Marie	Chippewa County International	P	N	22,261	20,974	6.14%			
341 GL MN BRD	Brainerd	Brainerd Lakes Regional	P	N	21,383	18,328	16.67%			
342 WP CA MMH	Mammoth Lakes	Mammoth Yosemite	P	N	21,352	21,826	-2.17%			
343 WP AZ PGA	Page	Page Municipal	P	N	21,347	17,435	22.44%			
345 WP MP GRO	Northern Islands (Municipality)	Benjamin Taisacan Mangiona International	P	N	21,214	19,178	10.62%			
346 GL MN STC	St. Cloud	St. Cloud Regional	P	N	20,918	15,615	33.96%			
347 SO KY PAH	Paducah	Barkley Regional	P	N	20,910	20,266	3.18%			
348 AL AK LHD	Anchorage	Lake Hood	P	N	20,689	23,382	-11.52%			
349 NM UT OGD	Ogden	Ogden-Hinckley	P	N	20,324	15,609	30.21%			
350 SO KY OWB	Owensboro	Owensboro-Daviess County Regional	P	N	19,578	23,537	-16.82%			
352 SW TX GGG	Longview	East Texas Regional Dickinson - Theodore Roosevelt Regional	P	N	19,261	22,480	-14.32%			
353 GL ND DIK	Dickinson		P	N	18,963	16,822	12.73%			
354 SO PR SIG	San Juan	Fernando Luis Ribas Dominicci	P	N	18,361	24,054	-23.67%			
355 EA PA IPT	Williamsport	Williamsport Regional	P	N	18,323	19,320	-5.16%			
356 AL AK MRI	Anchorage	Merrill Field	P	N	18,298	19,144	-4.42%			
357 AL AK CDV	Cordova	Merle K (Mudhole) Smith	P	N	18,280	18,649	-1.98%			
359 NM WY RKS	Rock Springs	Rock Springs-Sweetwater County	P	N	18,109	16,110	12.41%			
360 GL MI IMT	Iron Mountain	Ford	P	N	17,707	12,604	40.49%			
361 NM WA BFI	Seattle	Boeing Field/King County International	P	N	17,294	17,795	-2.82%			
362 WP NV EKO	Elko	Elko Regional	P	N	17,115	14,893	14.92%			
363 NE RI WST	Westerly	Westerly State	P	N	17,021	17,218	-1.14%			
364 GL MI MKG	Muskegon	Muskegon County	P	N	17,007	18,165	-6.37%			
365 GL MI ESC	Escanaba	Delta County	P	N	16,781	16,822	-0.24%			
366 SW NM HOB	Hobbs	Lea County Regional	P	N	16,216	14,707	10.26%			

Jack Brooks Regional Airport

Wildlife Hazard Management Plan

To comply with CFR 14 PART 139.337

As administered by the
Federal Aviation Administration

Alex Rupp
Airport Director

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Wildlife Hazard Management Plan 139.337

Jack Brooks Regional Airport (BPT), previously known as Southeast Texas Regional Airport and Jefferson County Regional Airport, (ICAO identifier KBPT), is a public use facility owned by Jefferson County. The airport is categorized as a FAR Part 139 facility.

The airport is located immediately east of Nederland, 3 miles northeast of Port Arthur, and 9 miles southeast of Beaumont, Texas in unincorporated Jefferson County.

A predominate amount of the daily operations are General Aviation, however the Airport is also served by American Airlines flying CRJ 200s.

Currently, land immediately adjacent to the airport on the east and southeast is the residential and business community of Nederland and Port Arthur. To the north is mixed non-specific use and the residential community of Beauxart Gardens. To the south is primarily agricultural, non-specific use with mixed business activities. To the west is primarily agricultural, wetland, and non-specific uses.

Entrance to the airport is via the airport access from Memorial Highway (TX 287/96/69) that runs immediately east of the airfield. Other significant roads surrounding the airport include West Port Arthur Avenue (TX 93) west of the airport and FM365, south of the airport.

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I. Purpose

The specific goals of this document are:

- 1) Assign responsibility to organizations and individuals for carrying out specific actions at projected times and places in responding to a wildlife strike.
- 2) Identify personnel, equipment, facilities, supplies, and other resources available, within the airport for wildlife assessment and management.
- 3) Facilitate response and short-term recovery to set the stage for successful long-term recovery.

II. Implementation

The following personnel are in charge of assisting and carrying out the instructions that are given to aid in the reduction of wildlife hazards when necessary: **Maintenance / ARFF / Fuel Service Personnel.**

All maintenance personnel and ARFF Captains in charge of making wildlife observations throughout the day and report any issues immediately to the Operations Manager or ARFF Chief.

- Observation reportable issues during normal hours are:
- Roosting wildlife
- Nesting areas in hangars
- Concentration of birds during mowing operation
- Birds roosting on perimeter fences
- Ground nesting in gravel areas or joints of concrete
- Standing or ponding water
- Assist in checking wildlife snares set by the state of Texas

III. State Law Enforcement

The State Wildlife Office aids the airport in wildlife hazard management. The State Wildlife Office provides, sets, and polices wildlife snare along the perimeter fence of the airport. The agent has access to the perimeter road through Gate #1 and #3.

IV. Federal Assistance

The Jack Brooks Regional Airport does not receive any Federal assistance in wildlife control. The airport intends to maintain a Migratory Bird Depredation Permit from the U.S. Fish and Wildlife Service. This permit allows controlling certain migratory birds specified in the permit on a year round basis.

V. Wildlife Attractants at Jack Brooks Regional Airport (BPT)

Per FAA Part 139 requirements, the BPT staff is responsible for ensuring that airfield habitat, surface water drainage, and perimeter fencing are managed to minimize bird and wildlife attractants.

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Preliminary observations of the AOA showed moderate interest and activity to minimize bird and mammal attractants. In addition, an excellent cooperative relationship exists between the airport, other airport users, and the airport staff is pursuing wildlife management techniques in accordance with the FAA Part 139 requirements.

The location of desirable habitat relative to the airport operations area (AOA) is also an important component in determining wildlife strike potential. When attractants are located on both sides of a runway or taxiway, wildlife will routinely travel between these areas during a daily period resulting in an increased strike hazard.

The airport and adjacent areas historically contained primarily tall/midgrass prairies along with wetlands, and flat wood forests, which are poorly drained flat sites that could have various different species mixtures. With the exception of numerous wildlife refuges and areas in the near vicinity of the airport, very little of this type of habitat still exists.

The current habitat consists largely of converted land communities with non-native grass species, annual forbs and woody plants, agricultural conversion, and oil business-related activities.

The current available wildlife habitat at the airport can be divided into four main groups: woodland/shrub land, water features, developed and disturbed sites, and agricultural land. Each one of these habitats at the airport has particular attractants that are responsible for sustaining potentially hazardous wildlife activity.

Vegetation is one attractant that all of these habitats have in common. Vegetation provides much of the food and cover requirements for wildlife. Mixed species of weeds, grasses, legumes, and fruit bearing woody plants provide food sources as well as nesting and roosting cover.

Woodland/shrub land habitat consists of both native and non-native species. The dominant species of trees on both the airfield and surrounding area include: Chinese Tallow (*Triadica sebifera*), oaks (*Quercus spp.*), Yaupon Holly (*Ilex vomitoria*), and willows (*Salix spp.*). The woodland/shrubland areas have a dense ground cover of Dewberry (*Rubus trivialis*) and Greenbriar (*Smilax spp.*). Wet marsh areas are commonly mixed throughout this cover type.

The south and southeast AOA and narrow stands along perimeter edges of the airport are lined by these woodland/shrub land habitats. These wooded areas adjacent to the airfield provide ideal habitat for many bird and mammal species, to include Coyotes, Bobcats, Feral Hogs, and raptors and contribute significantly to the potential wildlife risk on this airport.

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VI. Habitat Modifications and Changes

TASK	PROJECTED COMPLETION DATE:	ACTUAL COMPLETION DATE:
The Jack Brooks Regional Airport will make every attempt to change or modify the habitat to aid in the control of the wildlife on airport property. The airport is limited by the lack of jurisdiction over the privately owned property that surrounds the airport. The airport will attempt to inform the neighboring property owners whose property lies on runway approaches and encourage them not to enhance the habitat in these areas.	CONTINUAL	CONTINUAL
In the distance of two to five miles away from the airport various parcels of agriculture land and marshland exist. The airport will attempt to educate the owners of the agriculture land and encourage them to perform certain agriculture operations at night if at all possible. The marshland has been in existence for many years, the airport does not have the jurisdiction or the ability to modify.	CONTINUAL	CONTINUAL
The airport will, in accordance with FAA recommendations, keep the airfield turf maintained between 6-12 inches over the entire area to limit bird numbers and reduce maintenance costs. Grass will not be allowed to exceed 12 inches, when possible, and to go to seed; as such conditions will attract rodents and raptors/vulture predators. Selective application of herbicides may initially be necessary to eliminate weeds and allow grass to become reestablished in areas where it has been disturbed.	CONTINUAL	CONTINUAL
Once established, grass maintained at 6-12 inches will out-compete and thus reduce broad-leaved weed species. This will enable a reduction in the amount of broad-leaved herbicide applied to the field, if applicable. The last mowing of the growing season should be to top off the grass at 6 inches where it will dry and stand through the winter.	CONTINUAL	CONTINUAL
The airport removed three large brush piles located on the airport by having a contractor chip the piles into mulch.	09/30/2017	01/30/2019
The airport currently has two storm damaged hangars. Hangar 3 is scheduled to be demolished by 04/30/19 and Hangar 4 is scheduled to be demolished and replaced by a third-party by 06/30/19. With these hangars removed, we anticipate the reduction of possible nesting areas for birds and rodents.	06/30/2019	
Located on the west side of the airport are several acres of trees and brush that are scheduled to be removed which will reduce possible nesting areas for birds and rodents. The airport has established a timetable for the project, see Appendix 3. Update (Feb 2019): The Airport has been successful in obtaining a contractor with specialized tree removal equipment and over the last two years, we have removed approximately 36.5 acres of trees and brush. The remaining 73 acres of trees are scheduled to be removed over the next 3 years.	12/30/2022	

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VII. Immediate Action

The airport will do the following to aide in reduction of wildlife.

- Maintain grass in safety areas grass height no less than 5 inches
 - Maintain grass no less than 2 inches around Taxiways and Runway edge lights and sign boxes.
 - Maintain ramps and AOA areas free of small gravel
 - Maintain ramps and AOA areas joint seals clean of grass
 - Provide efforts to eliminate nesting areas for birds
 - Tractor operators will be supplied with pyrotechnics to disperse wildlife during mowing operations

VIII. Long term habitat modification

The short term goal of the airport is to remove all trees within the perimeter fence and the Runway 30 Approach property, see Appendix 3 for the Tree Removal Plan.

The long term goal of the airport is to maintain the areas of tree removal to restrict future tree growth.

IX. Wildlife permits

The Jack Brooks Regional Airport will obtain U.S. Fish and Wildlife Services - Migratory Bird Depredation Permits. The permit allows controlling certain migratory birds specified in the permit on a year round basis. The permitted birds are: Morning Doves, Cattle Egret, Killdeer, and Laughing Gull.

Active permits can be found in Appendix 4.

X. Pesticides

The Jack Brooks Regional Airport does not use pesticides to control wildlife.

XI. Airport Personnel / Equipment

The Jack Brooks Regional Airport will provide airport personnel necessary to control wildlife, maintain the airport, and to prevent or possibly reduce aircraft strikes. The airport has 8 full time maintenance personnel that are on duty between the hours of 6 A.M. to 4 P.M., Monday through Friday. The airport has 9 ARFF personnel that are on duty through various shifts 24 hours a day, 365 days a year.

Available Equipment / Supplies:

- 2 - Kubota tractors equipped with airfield radios
- 2 - 15Ft Rhino mower decks.
- 1 - 6ft Rhino mower
- 4 - ZTR mowers
- 1 - 1000.00 Gal spray rig with 50FT booms and gps marking system

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- 1 – Tymco Sweeper with airfield radio
- 8 – Pickup trucks with airfield radios
- 2 – ARFF trucks with airfield radios
- 1 – Club car ATV
- 1 - 12GA shotgun
- 4 - pyrotechnic launcher
- Several boxes of pyrotechnic shells (screamers, bangers)
- Primers for pyrotechnic launcher
- Several boxes of steel shot 12GA shotgun shells

All supplies are kept under lock and key, only accessible by the Operation Chief, ARFF Chief or Airport Director.

XII. Assignment of Personnel for Implementing the Plan

It is the duty of all airport personnel to report wildlife activity to the Operation Chief and at that point he will advise what needs to be done. If the Operation Chief is not available, then personnel are to contact either the ARFF Chief or Airport Director. If personnel are unable to contact any member of the management team, they have the authority to activate the plan to maintain a safe environment for the flying public.

XIII. Inspection for Wildlife

Throughout the day beginning at dawn airport personnel will make wildlife observations during daylight operations. During these observations personnel should focus on all ramps, AOA areas, and safety areas. At any time a significant amount of wildlife has been spotted it is to be reported. At any time ATC reports birds, personnel will immediately inspect the area to determine what action is needed, if any to correct the issue.

XIV. Wildlife Control Measures

When a wildlife hazard has been reported, an inspection will take place and the proper means of control will be made. The procedures listed below are to be followed to determine course of action.

- Identify the type of wildlife.
- Is it an endanger species?
- Do we have a permit for the species if lethal action is needed?

What is the easiest way to disperse the wildlife without doing harmful impact to the environment?

Once the procedure listed above has been vetted, then personnel shall proceed with whatever course of action listed in the plan to make the area safe.

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XV. Approved Action for Controlling Wildlife

Once the hazard has been identified, detraction can be made by the following actions:

- Contact ATC to see if ANY aircraft are about to land or take off.
- If you are on the ramps consider where is aircraft parked?
- First line of action is harassment- use horns, sirens, and water
- Second line of action- consults with ATC again on aircraft traffic; if clear then proceed with pyrotechnics. (screamers and bangers)
- Third line of action is lethal action- consults with ATC again on aircraft traffic, identify what is behind your target, and determine if it is safe to discharge a firearm?
 - Point the weapon in a safe direction and load the weapon with NON TOXIC STEEL SHOT.
 - Double check the area to ensure the area is safe to precede, if safe discharge the firearm.
 - Once lethal action has been taken any type of wildlife is to be removed and disposed of in a proper manor. The employee shall wear GLOVES to prevent any type of exposure to diseases that the animal may have.
 - Once the hazard no longer exists, relay the information to the ATC and airport Management.

XVI. Training

The JBRA has a training program that is a regular part of our 139 training. This training is done a minimum of every 12 consecutive calendar months. The training is also reviewed if a triggering event occurs.

XVII. Evaluation and Review of the WHMP

The airport will review the WHMP a minimum of once every 12 calendar months and will be a part of the regular plan review cycle at the airport. The airport will hold an annual meeting to discuss any other hazards that may have arisen to become an issue. If an aircraft incident occurs it will automatically trigger a review of the plan.

When the annual review is being performed the following form will be used to aid in the review.

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Appendix 1. WHMP Annual Review

Date: _____

Subject: Wildlife Hazard Management Plan Annual Review

Airport: JACK BROOKS REGIONAL AIRPORT

Airport ID: BPT

On _____ we conducted the annual review the Wildlife Hazard Management Plan, as per the requirements of 139.337 (f) (6).

General Information/Significant findings:

Name of review coordinator : _____ (Person facilitating discussions and writing plan updates; usually the Wildlife Coordinator, Wildlife Biologist, or Airport Director) & participating airport personnel and representatives of other organizations (As listed in 139.337 (f) (1); may include members of airport management, the wildlife coordinator, airport operations/wildlife staff, wildlife Biologist who conducted Wildlife Hazard Assessment, members of the wildlife hazard working group*). **Attach a sign-in sheet.**

Summary of results of annual data analysis:

- -
- -
- -
- -
- -
- -

Example: ranking of highest priority species based on the analysis. (Per standardized continual monitoring procedures of 139.337 (f) (6); data for analysis may include logs of wildlife strikes, wildlife observations and control measures, standardized wildlife monitoring surveys, and wildlife data from off – airport sites of concern.)

Summary of progress and challenges in management of the most significant wildlife attractants and / or habitats on or near the airport – (Review of habitat management proprieties listed in 139.337 (f) (2)).

Summary of progress and challenges in direct wildlife hazard management (i.e., dispersals, strike response) on the airfield – (Review of procedures to be followed during air carrier operations as listed in 139.337 (f)(5)).

Changes to management strategies identified and changes to documentation identified.

Changes to Wildlife Hazard Working Group membership or objectives identified and changes to airport training program identified.

Changes/ updates to Wildlife Hazard Management Plan Identified

(Submit any changes to the WHMP to the assigned FAA Airport Certification Safety Inspector)

Airport Operations Manager

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Appendix 2. WHMP Review Following a Triggering Event

Date: _____

Subject: Wildlife Hazard Management Plan Review Following a Triggering Event

Airport: JACK BROOKS REGIONAL AIRPORT

Airport ID: BPT

On _____ we conducted a review the Wildlife Hazard Management Plan, as per the requirements of 139.337 (f) (6).

Description of Triggering Event:

Date/Time – Provide details of the event which triggered the review. Attach strike report, if available and any pertinent information; runway used, airline, take-off, landing, species, damage, etc.

General Information/ Significant findings:

Name of review coordinator – (Person facilitating discussions and writing plan updates; usually the Wildlife Coordinator, Wildlife Biologist, or Airport Director) & participating airport personnel and representatives of other organizations (As listed in 139.337 (f) (1); may include members of airport management, the wildlife coordinator, airport operations/ wildlife staff, wildlife Biologist who conducted Wildlife Hazard Assessment, members of the wildlife hazard working group*). Attach a sign – in sheet.

The plan's effectiveness in dealing with known wildlife hazards on and in the airport's vicinity – Example: Review the current wildlife control log and evaluate recent strike reports or events. Make a determination as to whether the current program is working and what can be improved.

Aspects of the wildlife hazards described in the wildlife hazard assessment that should be reevaluated – Review assessment to determine if everything is being addressed that was previously identified as a hazard or if other species are now present. Note: if other/additional new species are now present on or in the vicinity of the airport, another Wildlife Hazard Assessment may be needed.

Summary of progress and challenges in direct wildlife hazard management (i.e. dispersals, strike response) on the airfield – (Review of procedures to be followed during air carrier operations as listed in 139.337 (f)(5))

Changes to: management strategies identified, airport training program identified

Changes/updates to Wildlife Hazard Management Plan identified

(Submit any changes to the WHMP to the assigned FAA Airport Certification Safety Inspector)

Airport Operations Manager

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Appendix 3. Tree Removal Plan



The tree areas shaded in orange above were removed Oct 2017 (11.2 acres)

The tree areas shaded in white above were removed Dec 2018 (22.5 acres)

The airport plans to remove the red areas (17 acres) Dec 2019, blue area (37.5 acres) Dec 2020, and then the green area (19.2 acres) Dec 2021.

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Appendix 4. US Fish and Wildlife Depredation Permit

(Attach Current Depredation Permit Behind This Page)

Original Date: _____

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